



Parkview (East of 142 Street) Neighbourhood Renewal

Edmonton

URBAN DESIGN ANALYSIS REPORT
CHAPTER 1: NEIGHBOURHOOD BACKGROUND REPORT

APRIL 2026

Introduction

Neighbourhood Renewal

As neighbourhoods mature, their infrastructure naturally ages and requires renewal to ensure it can continue serving the community well into the future. The City of Edmonton, through its Neighbourhood Renewal Program, is committed to more than simply replacing aging infrastructure. This program also provides a meaningful opportunity to work with residents to identify improvements that can enhance neighbourhood vibrancy, sustainability, and long-term resilience.

Urban Design Analysis

The background phase of an Urban Design Analysis (UDA) establishes the foundation for any Neighbourhood Renewal project. Its purpose is to develop a comprehensive understanding of the community and the many factors that shape it. This includes social and economic demographics, the public realm (such as parks and green spaces), existing transportation and active mobility networks, applicable policies and guidelines, and the neighbourhood's urban form and land use patterns. This work is guided by the City's strategic direction and planning framework, which shape the approach to Neighbourhood Renewal.

During this phase, the project team compiles detailed background information—from historical records to technical data—and brings this knowledge together in a way that is accessible to the community. Developing a comprehensive inventory of neighbourhood characteristics and analyzing this context establishes a shared baseline that supports informed planning. This understanding enables both the project team and residents to engage in meaningful discussions about the neighbourhood's identity, needs, and priorities, and how renewal can support a resilient, inclusive, and adaptable neighbourhood in the decades ahead.

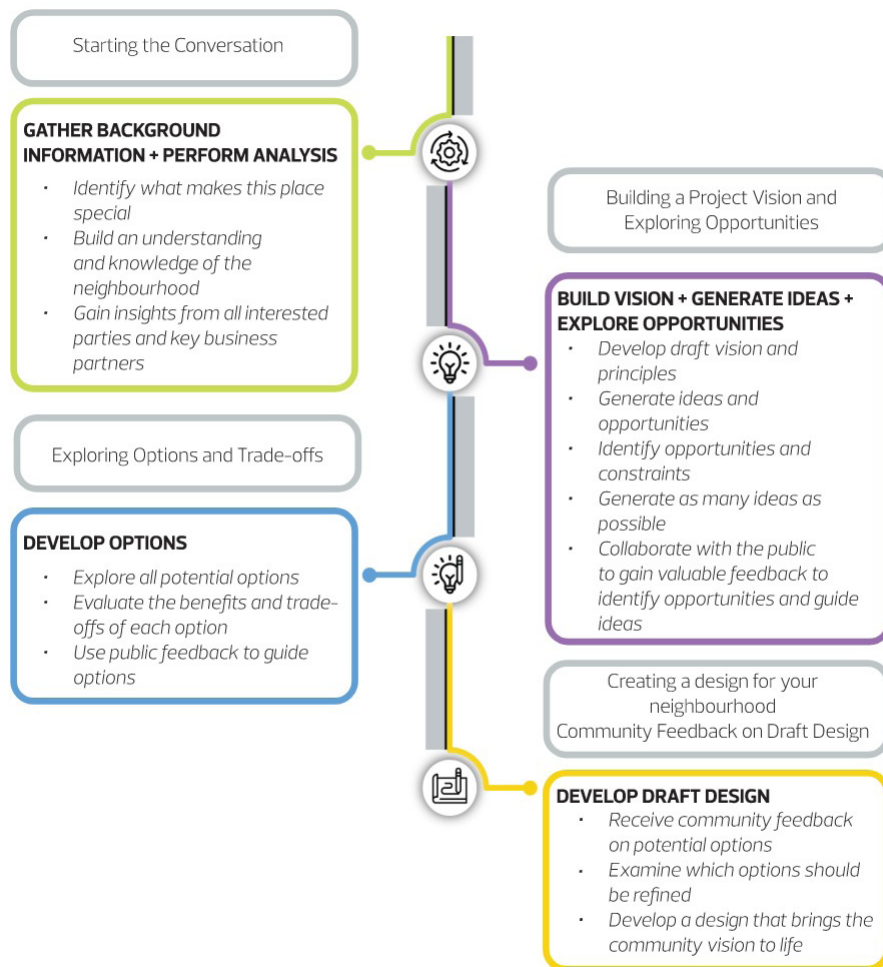
Through collaboration with residents and other interested parties, the project team will gain insight into local values, lived experiences, and long-term aspirations. Combined with policy alignment and technical analysis, renewal prioritizes safety, accessibility, and equity for residents of all ages and abilities.

Ultimately, this holistic and policy-aligned approach guides renewal of neighbourhood infrastructure to address current servicing needs, future growth, evolving mobility patterns, climate resilience goals, and changing demographic trends. By planning with a 35+ year horizon, the City aims to ensure that infrastructure renewal in Parkview East delivers durable, adaptable, and fiscally responsible improvements that serve both current residents and future generations.



Project Approach + Process

The Parkview (East of 142 Street) UDA is part of the Concept Phase of the City's Neighbourhood Renewal Road Map. This phase includes sharing information about the Neighbourhood Renewal Program, establishing a neighbourhood's vision for livability and mobility, and analyzing existing urban design conditions to identify key strengths and opportunities. This information informs a step-by-step process that includes learning about the neighbourhood, developing ideas together, and creating design options, which lead to a draft design that highlights the key opportunities identified.



Neighbourhood Background + Context

Study Area

Neighbourhood Location + History

Parkview (East of 142 Street) comprises a portion of the broader residential community of Parkview in the City of Edmonton. Situated along 142 Street and Buena Vista Road, the area is framed by MacKenzie Ravine to the north and the North Saskatchewan River to the east. While part of the larger Parkview neighbourhood, which shares boundaries with Laurier Heights, Crestwood, Sherwood, and Jasper Park, 142 Street is a significant north–south arterial road which creates a strong physical barrier that separates Parkview (East of 142 Street) from the remainder of the neighbourhood to the west.

Parkview was developed in the mid to late 1950s and is characterized by its predominantly single-detached homes and its scenic views of the North Saskatchewan River Valley. The portion of Parkview (East of 142 Street) – formerly known as Valleyview and now includes parts of the former Buena Vista neighbourhood – is the focus for this Urban Design Analysis.

Neighbourhood Renewal in the portion of Parkview west of 142 Street was completed in the early 2000s. Now, the areas within Parkview that are east of 142 Street are the focus of this current Neighbourhood Renewal project. For simplicity, we will refer to the study area as Parkview East in this document. However, we note that this is not a separate neighbourhood but only a portion of the Parkview neighbourhood.



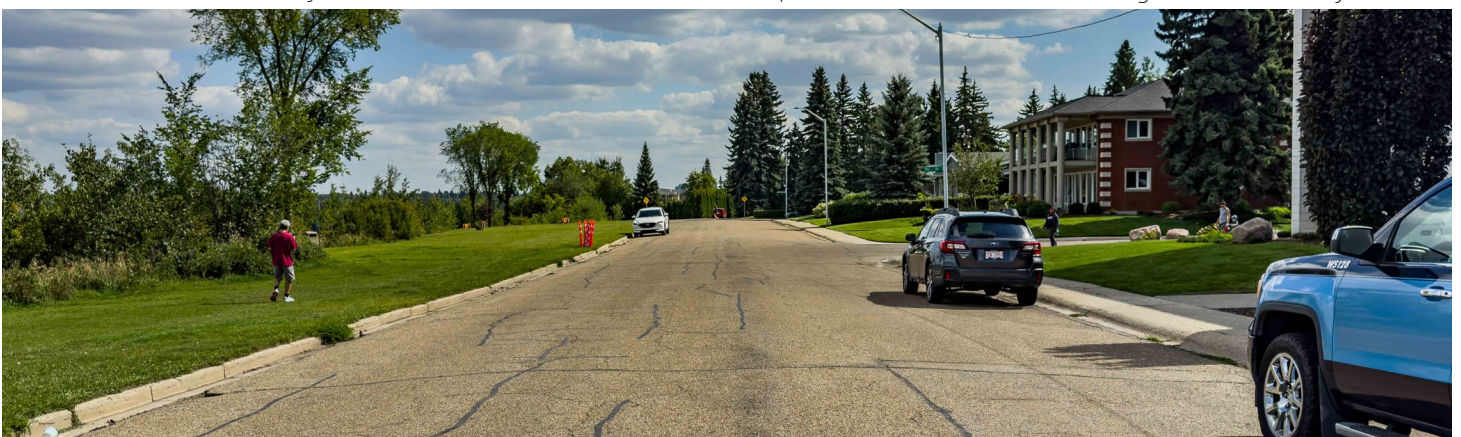
Residential infill on 141 Street



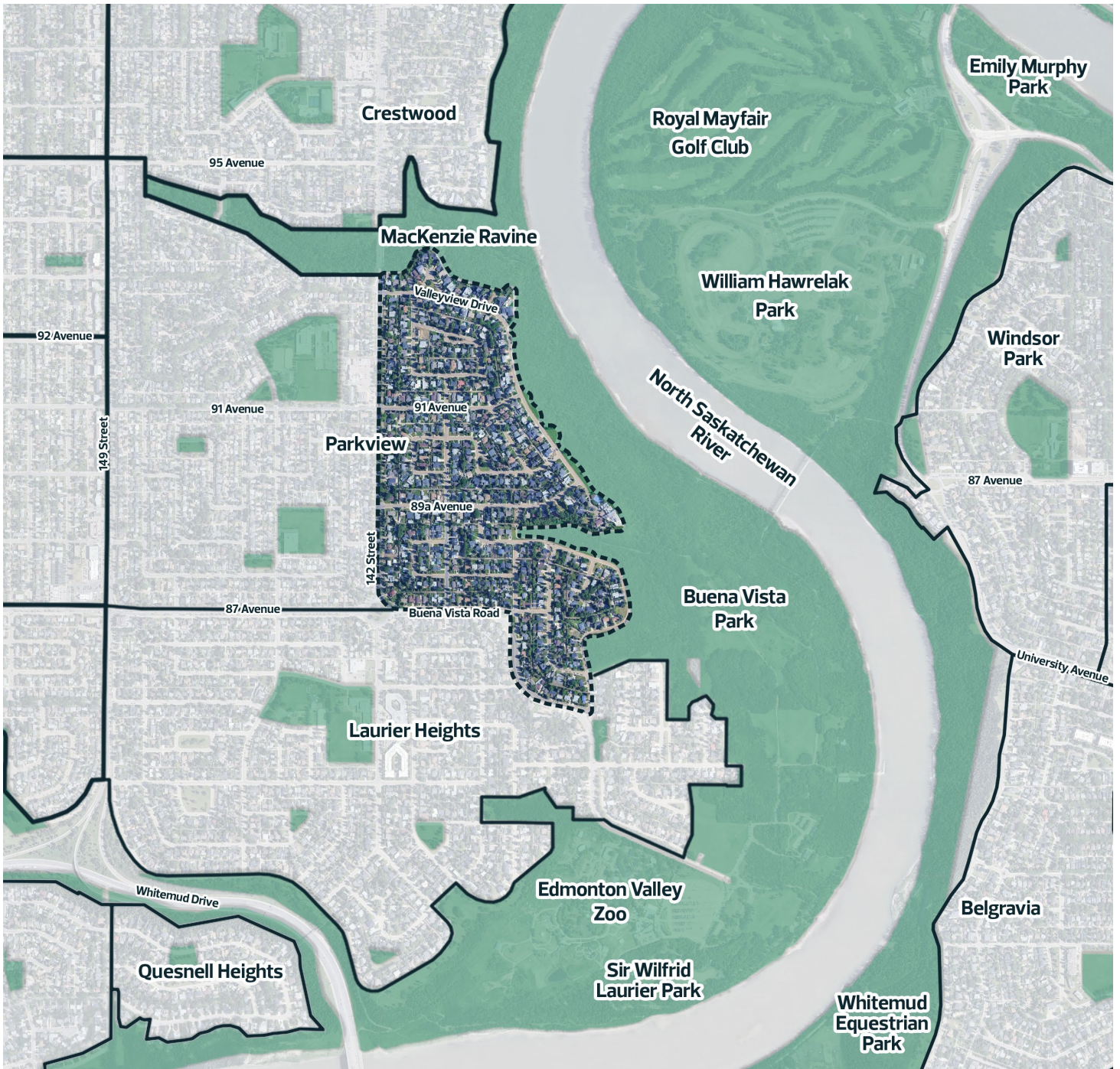
Sidewalk repairs on 141 Street



Missing sidewalks on Valleyview Point



Valleyview Drive and open space along the North Saskatchewan River Valley



Map 1 – Neighbourhood Context



Map Legend

- Project Area
- Open Spaces
- Neighbourhood Boundary

Scope

The Neighbourhood Renewal project focuses on Parkview East. The project area is bounded by MacKenzie Ravine to the north and the North Saskatchewan River Valley to the east. The southern boundary follows Buena Vista Road between 142 Street and Valleyview Crescent. The study area's western boundary is 142 Street.

The project scope includes all local roads and their associated infrastructure, such as driving surfaces, curbs, street lights, and sidewalks. Alleys and arterial roadways are not included, as they are addressed through separate City programs. An important scope feature of this study is the distinction between the project area and the broader area of influence. While construction is limited to the project area, the study also considers the surrounding area of influence to better understand how the neighbourhood connects to adjacent transportation networks and key destinations. The area of influence generally includes walkable distances beyond the project area and important nearby destinations. As part of the Neighbourhood Renewal Program, opportunities are explored to strengthen connections to active transportation networks within the project area and between adjacent neighbourhoods.

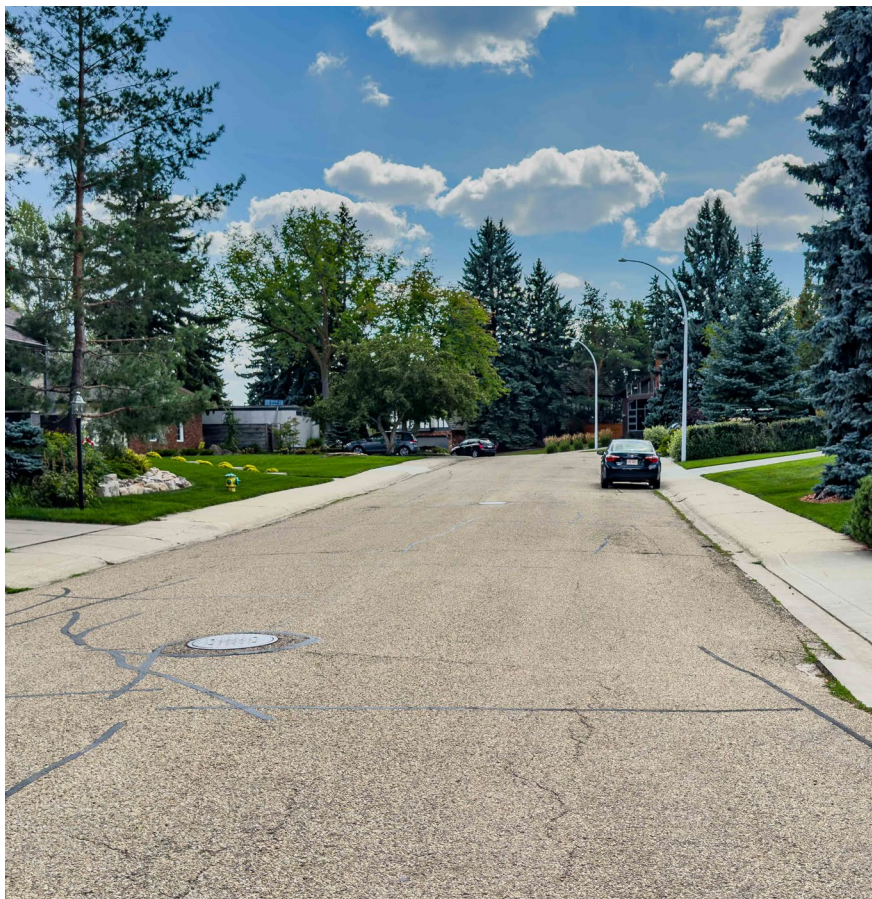
In addition to roadway improvements, Neighbourhood Renewal includes addressing missing links in the active modes network, which can include routes through open spaces and amenities that support the routes such as trees for shade, and seating. This may involve revitalizing existing amenities and assessing opportunities to enhance their functionality and usability for residents.



Asphalt failure along 142 Street service road



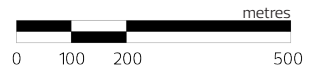
142 Street service road







137 Street



Map 2 – Project Scope



Map Legend

-  Project Area
-  Roadway Greens
-  Local Road
-  Service Road

Physical Context

Land Use + Built Form

Parkview East's built form reflects the characteristics of many post-war neighbourhoods in Edmonton.

Predominantly composed of single-detached homes, Parkview East is a low-density residential environment defined by a modified grid street pattern and a collection of small roadway greens within road right-of-way. There are no commercial or retail land uses within the project area.

Consistent with development of the era, Parkview East features relatively wide streets and narrow curbside sidewalks, with few internal green spaces. The neighbourhood's most significant public realm asset is the top-of-bank open space along Valleyview Drive, where residents enjoy panoramic views of the North Saskatchewan River Valley along with access to river valley trails.

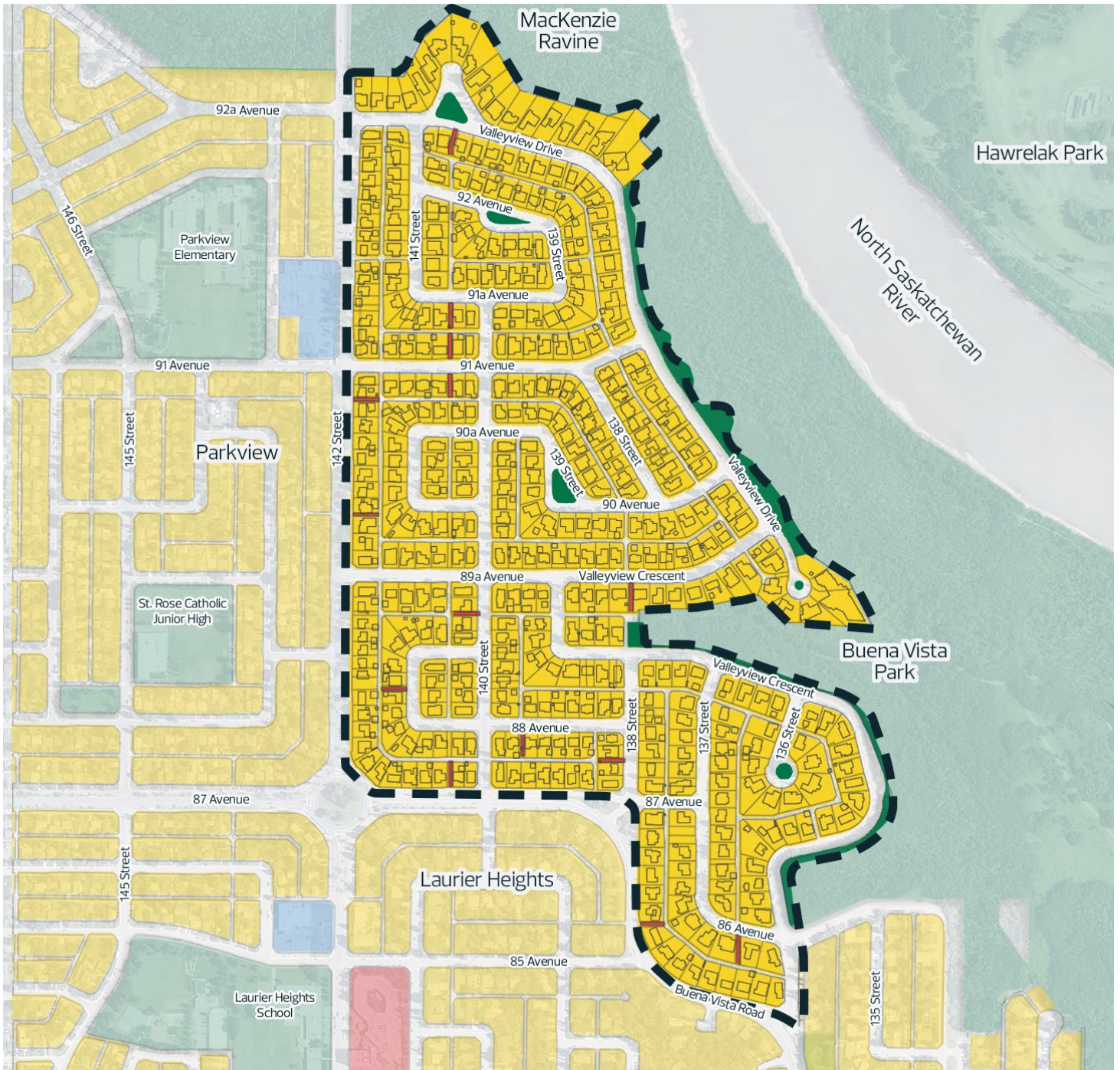
Planned + In-progress Development

There are multiple residential properties currently under construction, ranging from single-detached infill developments to renovations of existing homes. Parkview East also contains a significant number of existing large-scale infill homes.

Immediately outside the project area, there are plans to renew recreational infrastructure within MacKenzie Ravine, including trail networks, staircases, and viewing platforms.









90A Avenue



Map 3 – Land Use



Map Legend

-  Project Area
-  Open Spaces
-  Residential
-  Public Utility Lot
-  Commercial
-  Institutional

Social Context

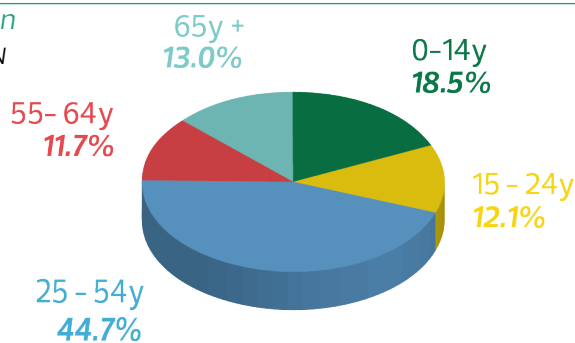
Demographics

This section provides an overview of the demographic characteristics of the Parkview neighbourhood, both east and west of 142 Street, including population, housing, employment, household composition, income, and travel behaviour. Understanding these trends, and how they compare to city-wide patterns, helps inform the Urban Design Analysis and supports Neighbourhood Renewal decisions that respond to current conditions while planning for long-term change.

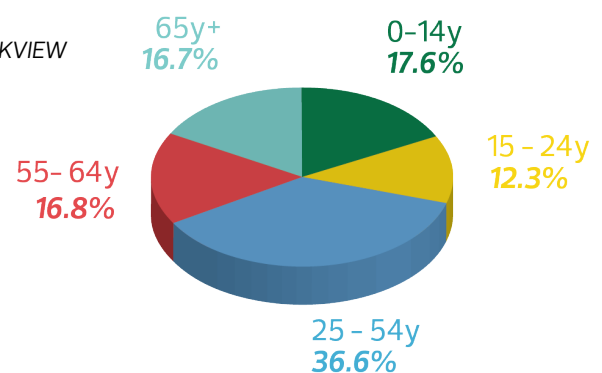
The data provided is sourced from 2021 Statistics Canada data (Federal census), which is the most current available dataset.

Population

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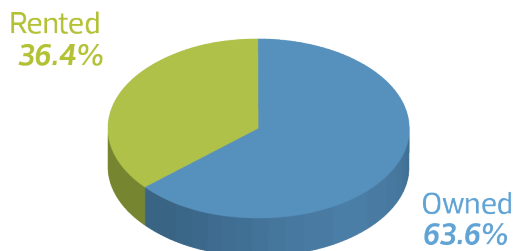


As of 2021, Parkview had a population of 3,375 residents in about 1,400 households. Of these, 36.6% were of working age (25 to 54 years old), while 16.8% were in the pre-retirement age group (55 to 64 years old). Seniors aged 65 and older accounted for 16.7% of the population. The remainder of the community consisted of children and youth between 0 and 24 years of age. These population numbers are for the entire neighbourhood of Parkview. It is estimated that about 36% of the neighbourhood (about 1,200 people) live in Parkview East.

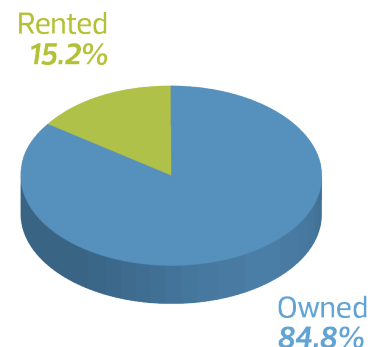
Over the past 50 years, Parkview's population has declined. In 1971 the neighbourhood had a recorded population of 5,735 residents, compared to 3,375 residents in 2021. This is a decrease of around 40% – about 2,360 residents.

Ownership

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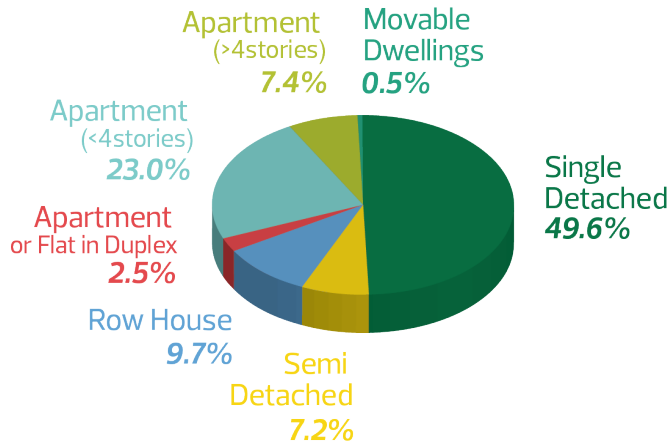
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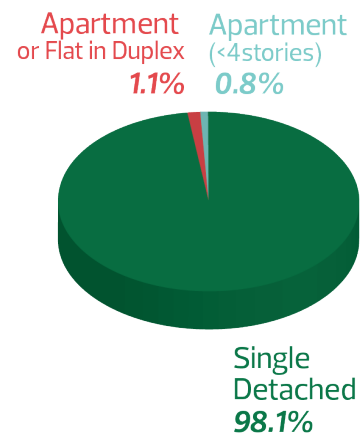
Parkview has a significantly higher proportion of homeowners compared to the city-wide average, with 84.8% of dwellings being owner-occupied versus Edmonton's 63.6%. Conversely, only 15.2% of homes are renter-occupied, compared to 36.4% across the city.

Housing Type

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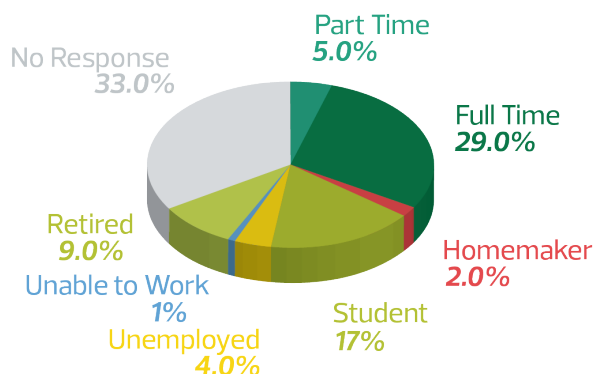


Residents of Parkview predominantly live in single-detached homes, with only a small proportion residing in apartments. All apartments in Parkview are located west of 142 Street. With very limited multi-family housing options, and a share of single-detached homes more than double the city-wide average, the neighbourhood is defined by a distinctly low-density residential character.

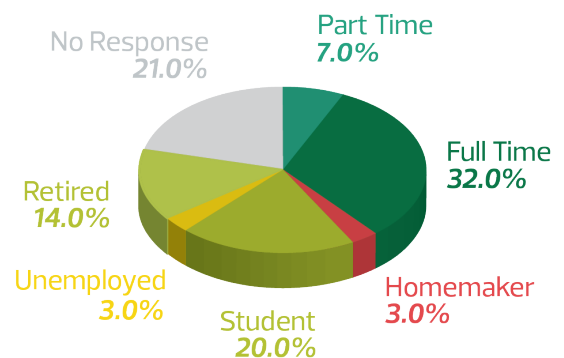
A comparison of population density across Edmonton neighbourhoods reveals a stark contrast between central and suburban areas. Wihkwêntôwin (formerly Oliver) is the densest neighbourhood in Edmonton, with over 100 people per hectare - which reflects its compact, mixed-use urban form close to downtown. Strathcona also shows substantially higher density (57 people/ha) than suburban counterparts. Population densities are a bit lower among newer and developing neighbourhoods such as Glenriding (46 people/ha) and Silver Berry (37 people/ha), indicating moderate suburban density relative to more traditional mature suburbs. Established low-density neighbourhoods such as Parkview (22 people/ha), Crestwood (20 people/ha), and Laurier Heights (14 people/ha) are significantly less dense. This pattern reflects differing housing forms, land use intensities, and eras of development. Inner-city, mixed-use communities concentrate the most residents per hectare, with newer suburban developments having moderate density. However, post-war suburban areas like Parkview extend over larger land footprints with the lowest population density.

Employment

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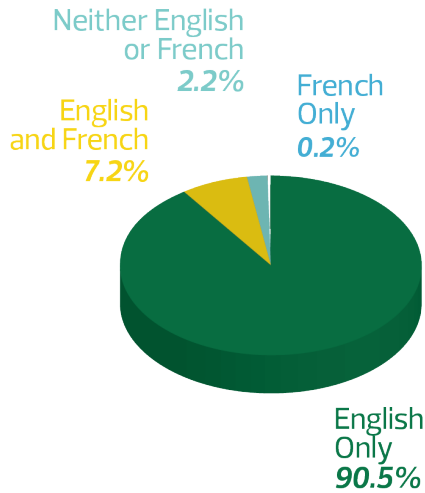


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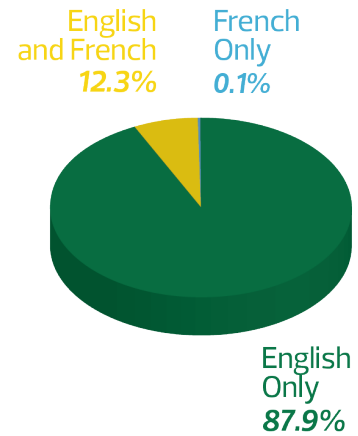


Of those who provided answers, Parkview has slightly higher proportions of both part-time and full-time workers compared to city-wide averages, along with higher shares of homemakers and students. The neighbourhood also has a larger retired population, at 14% compared to 9% across Edmonton.

Language
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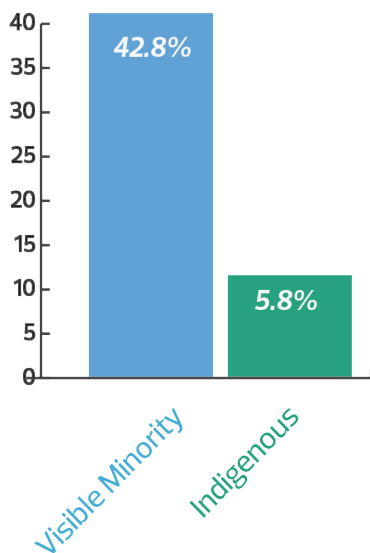
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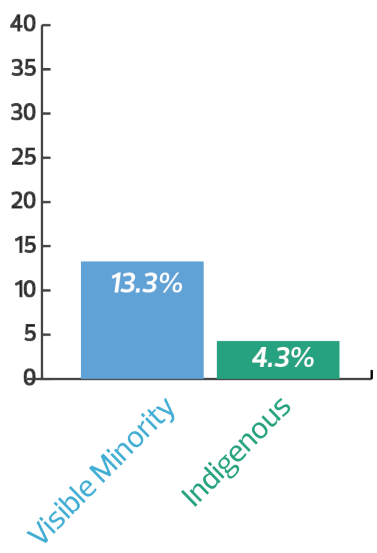
About 87.9% of the residents in Parkview speak English only, which is only slightly less than Edmonton's average of 90.5%. The population of Parkview that speaks English and French is 12.3%, which is more than Edmonton's average of 7.2%. The population that speaks only French is 0.1% in Parkview while it is 0.2% in Edmonton.

Visible Minorities + Indigenous Peoples

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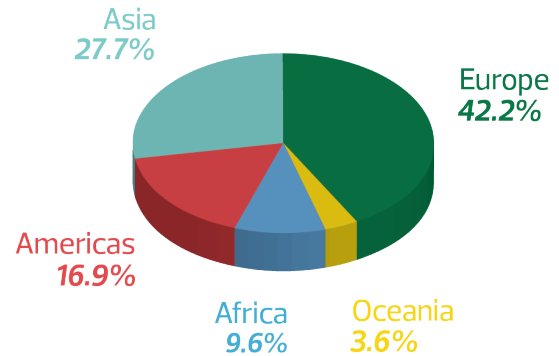
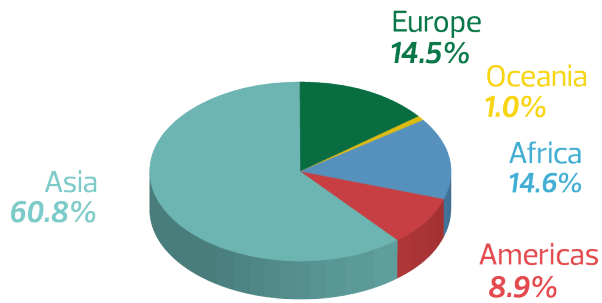


As of 2021, 13.3% of Parkview's population of 3,375 identify as Visible Minorities, and 4.3% identify as Indigenous. In comparison, of all people in the City of Edmonton, 42.8% identify as Visible Minorities, and 5.8% identify as Indigenous.

Immigrant Population Birthplace

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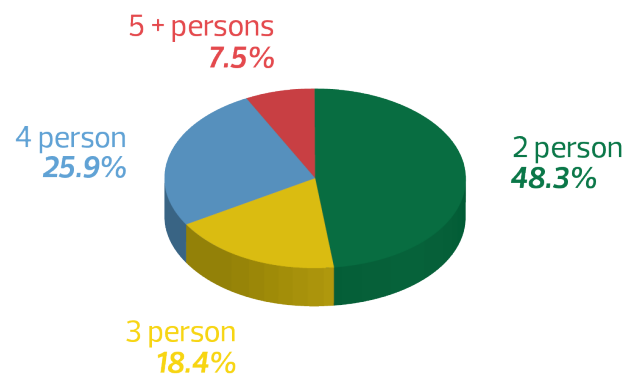
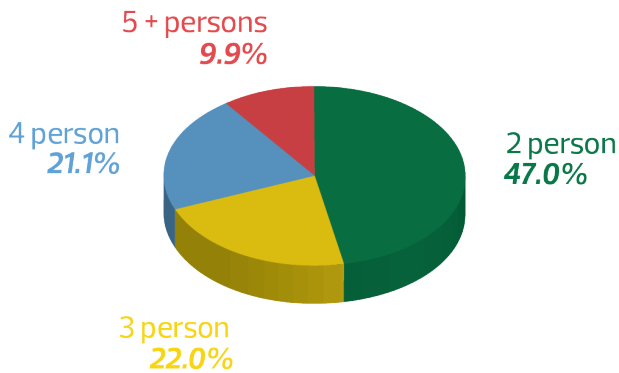


Among non-Canadian-born residents in Parkview, 42.2% were born in Europe, compared to 14.5% across Edmonton. Parkview has 27.7% of its non-Canadian-born population identify Asia as their place of birth, while Edmonton's average is 60.8%. The Americas are the birthplace for 16.9% of the non-Canadian-born population of Parkview compared to the Edmonton average of 8.9%. In Parkview, 9.6% and 3.6% of non-Canadian-born residents call Africa and Oceania, respectively, their birthplace. This compares to the Edmonton average of 14.6% and 1.0% of non-Canadian-born residents calling Africa and Oceania, respectively, their birthplace.

Family Size

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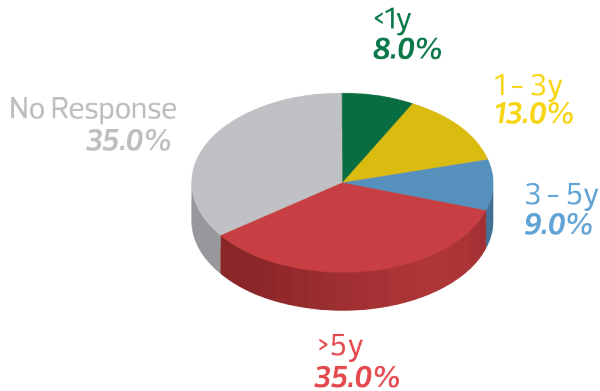
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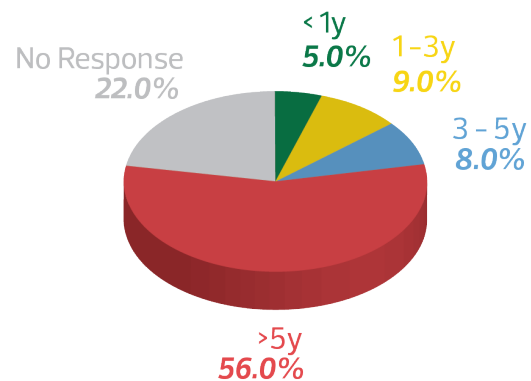
As of 2021, 48.3% of Parkview families consisted of two persons while Edmonton had 47.0% two person families. Three person families were 18.4% of families for Parkview and 22.0% for Edmonton. Census data indicates 25.9% of families in Parkview consisted of four people while Edmonton shows a lower percentage at 21.1%. Large families of 5 or more persons are the smallest proportion of the neighbourhood with 7.5% for Parkview compared to 9.9% for the City of Edmonton.

Length of Residency

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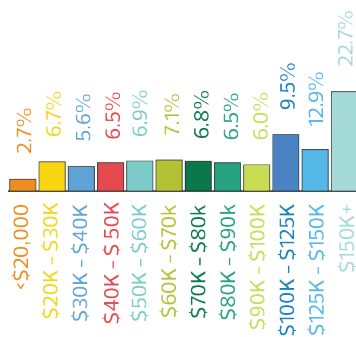
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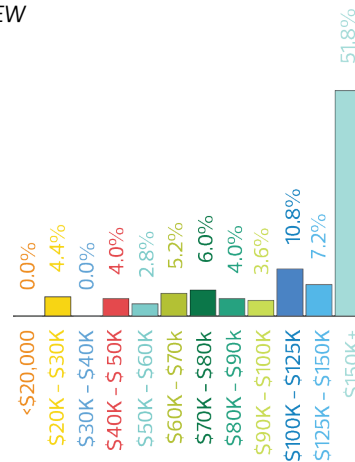
Parkview residents generally stay in their homes longer than the city-wide average. 56.0% of residents have lived in their homes for more than five years, compared to Edmonton's average of 35.0%. The neighbourhood also has lower proportions of residents in all shorter tenure ranges than the City overall.

Household Income

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PARKVIEW



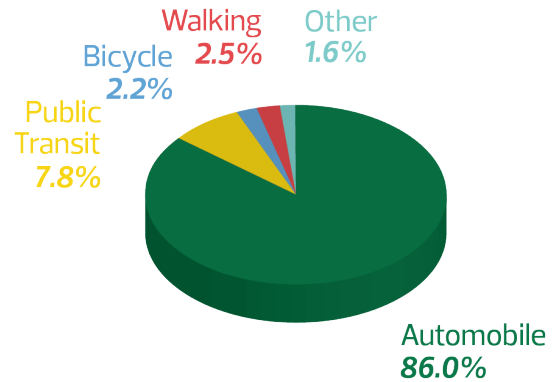
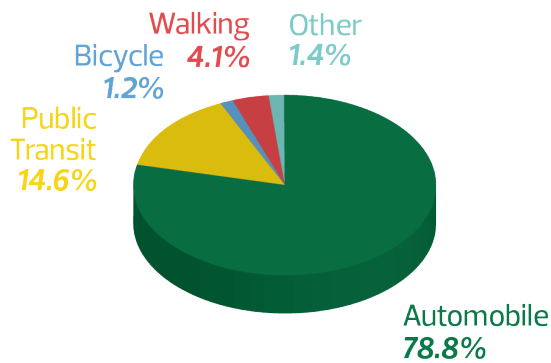
Parkview is an affluent neighbourhood, with a significantly higher proportion of households earning \$100,000 or more annually compared to the city-wide average. The highest income bracket (households earning \$150,000 or more per year) is represented in Parkview at more than two times the rate seen across Edmonton as a whole.

For the Parkview neighbourhood, 2021 statistics indicate that 13.7% of owner and tenant households spend more than 30% on household shelter costs. This compares to the City of Edmonton rate of 24.3%, indicating that households within Parkview are likely more financially comfortable than the City average. Another measure of financial comfort is the prevalence of low-income based on the Statistics Canada data of Low-income Measure, After Tax (LIM-AT). Generally speaking, this statistic is calculated using the median after tax income of households and the number of people per household. The LIM-AT measure for Parkview neighbourhood is 6% while the measure for the City of Edmonton is 10% indicating there is a smaller percentage of low-income households in the neighbourhood compared with the City overall.

Primary Mode of Transportation

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Parkview is an automobile-centric neighbourhood, reflecting the broader travel trends of Edmonton as a car-oriented city. The proportion of residents who commute by public transit or by walking is nearly half the city-wide average. Despite its vehicle-oriented nature, Parkview has a higher percentage of residents who commute by bicycle compared to Edmonton overall.

The 2021 federal census transportation data should be interpreted with caution, as it was collected during an unprecedented period shaped by COVID-19 disruptions. Widespread work-from-home arrangements, remote schooling, staggered schedules, and public health restrictions significantly altered typical travel behaviour, reducing commute trips and shifting mode choices in ways that may not reflect long-term or baseline conditions. Transit ridership declines, increased local trips, and suppressed peak-hour travel during this period were largely responses to temporary public health measures rather than durable changes in land use or transportation infrastructure. As a result, using 2021 census transportation trends as a primary basis for forecasting or design decisions may misrepresent post-pandemic travel patterns, underscoring the need to supplement this data with more recent counts, longitudinal trends, and qualitative insights.

PROJECT CONSIDERATIONS

Design for a maturing, predominantly long-tenure population while supporting inclusivity and accessibility by applying Access Design Guide, GBA+, and Winter Design Guidelines to improve walkability, winter comfort, and safe mobility for seniors, pre-retirement adults, and families. This includes ensuring barrier-free routes, comfortable crossings, high-quality lighting, and public realm elements that support year-round usability.

Provide a more sustainable and connected mobility system in an auto-oriented, low-density area by integrating principles from The City Plan, Complete Streets Design and Construction Standards, Safe Mobility Strategy, The Bike Plan, and Climate Resilient Edmonton. This includes enhancing active transportation links, improving biking and pedestrian infrastructure, and creating safe, convenient alternatives to driving while ensuring streets remain resilient, well-shaded, and able to function through changing climate conditions.

Historical Context

Pre Settlement and Early Land Use

Before urban development, the land that is now Parkview formed part of the North Saskatchewan River valley and plateau traditionally used by Indigenous peoples, including Cree, Saulteaux, Nakota Sioux, Blackfoot, and Métis communities. The river valley and adjacent ravines were important transport routes and seasonal gathering areas long before Edmonton became a city.

In the late 19th and early 20th centuries, as Edmonton expanded from a fur trade post to a growing town and then provincial capital (1905), the area remained largely rural and wooded, used for small-scale agriculture, resource extraction (such as gravel), and informal recreation along nearby ravines and within the river valley.

Post-War Residential Development (1950s–1960s)

The principal period of Parkview's development coincided with Edmonton's post-Second World War population boom. Returning veterans, a rising birth rate, and rapid economic growth (somewhat tied to oil and gas after the 1947 Leduc oil discovery), created strong demand for new suburban housing.

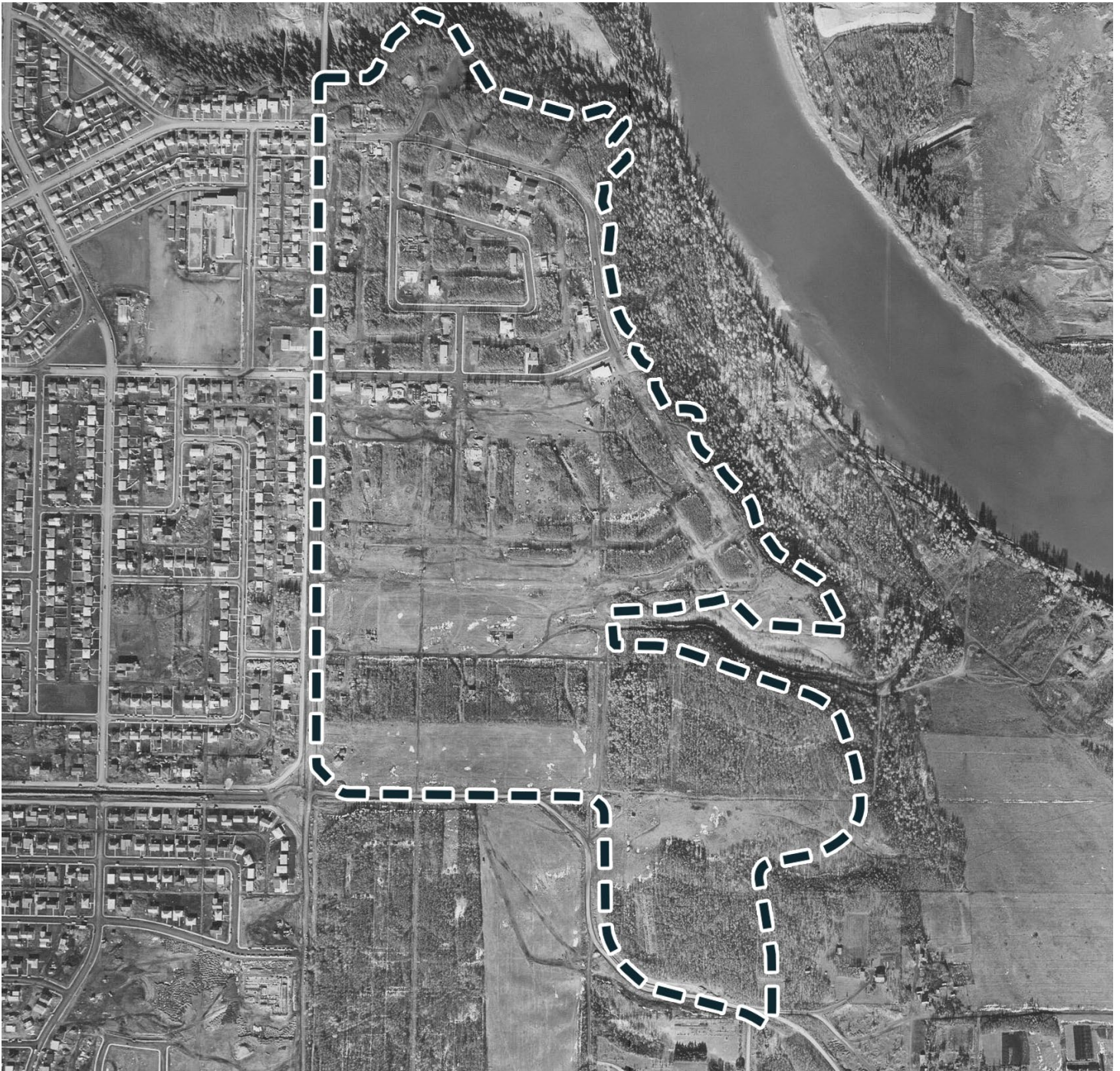
Historic Resources

Lieberman Residence: 13807 – Valleyview Drive

Built in 1958, the Lieberman Residence contributes to the historical narrative of Parkview's evolution from early post-war neighbourhood planning to its development into a prestigious residential area overlooking the river valley. Its inclusion in the City's historic resources inventory helps preserve the story of how Edmonton's mid-20th-century suburbs emerged and how architectural and community patterns shifted during that time. This home was designed and originally owned by David Lieberman, whose work is considered a leading example of the City's modernist architecture.

PROJECT CONSIDERATIONS

Neighbourhood Renewal in Parkview East should consider the area's deep historical roots and evolving identity. The project area lies within lands traditionally used by Indigenous peoples, including Cree, Saulteaux, Nakota Sioux, Blackfoot, and Métis communities; design, engagement, and communications should acknowledge this history. The neighbourhood's post-war development pattern, including mature trees and mid-century homes, is an important part of Parkview's sense of place. Renewal design should consider established elements and features, such as mature trees, viewpoints to the river valley, and recreational use of MacKenzie Ravine and adjacent green spaces. Where feasible, preservation of urban tree canopy and connections to the river valley should be prioritized. Design elements near the river valley system should consider their historic and ongoing recreational significance.



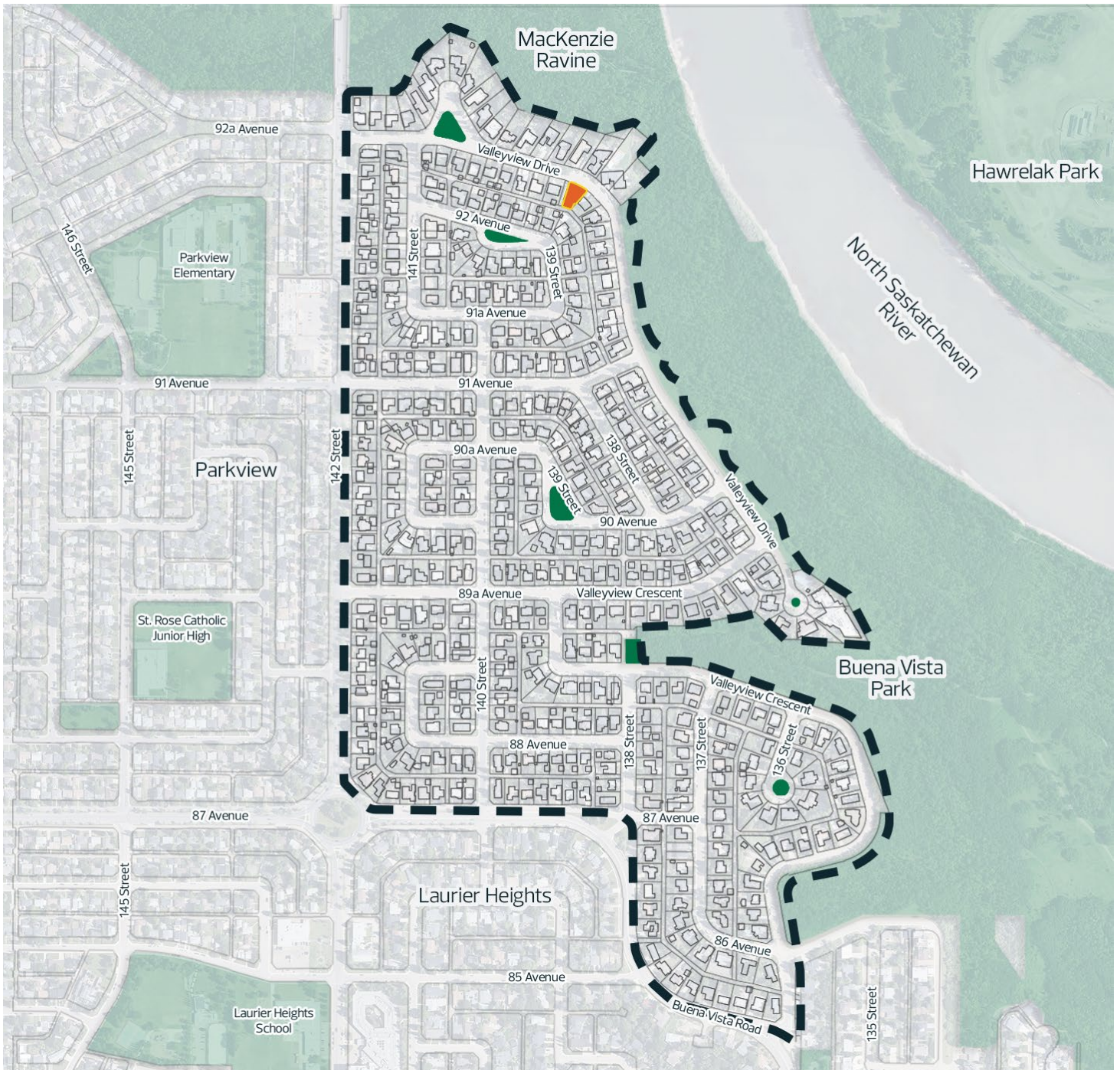
Map 4 – Historic Air Photo (1957)

Imagery courtesy of City of Edmonton Archives

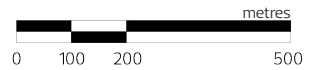
Map Legend

— — — Project Area








Map 5 – Historical Resources



Map Legend

-  Project Area
-  Historically Significant Property
-  Roadway Greens

Community Groups & Associations in the Parkview Neighbourhood

Parkview is supported by a number of community-driven organizations that provide programs, advocacy, and services for residents.

While the groups and organizations listed below generally operate outside the Parkview East project area, they serve residents within it. Ensuring accessibility to these destinations will be an important consideration when planning infrastructure improvements.

Parkview Community League

The Parkview Community League is the primary neighbourhood association and a member of the Edmonton Federation of Community Leagues (EFCL). It plays a central role in local life by:

- Operating the Parkview Community Hall and grounds (including sports facilities and green space).
- Offering recreation and social programs (e.g. youth sports, fitness classes, playgroups, seniors' activities, community events).
- Coordinating volunteer opportunities and neighbourhood initiatives such as clean ups, seasonal celebrations, and fundraisers.
- Coordinating with the City on community initiatives through their assigned Neighbourhood Resource Coordinator (NRC).

The League is governed by an elected volunteer board and supported by community volunteers.

Local Sports and Recreation Associations

Several sports and recreation groups serve children, youth, and adults in Parkview and surrounding west end neighbourhoods, often in partnership with the Parkview Community League:

- Minor Soccer Associations – Community-level outdoor and indoor soccer programs, typically organized through the community league and regional soccer zones.
- Hockey and Skating Clubs – Use nearby arenas and the Parkview outdoor rink for learn to skate programs, shinny, and organized hockey.
- Baseball/Softball Associations – Seasonal leagues and skills programs, drawing participants from Parkview and adjacent communities.

These groups promote physical activity, teamwork, and social connection for families in the area.

School-Based Parent and Community Councils

Public and separate schools serving Parkview (such as Parkview School and nearby elementary/junior high schools) host:

- Parent Advisory Councils / School Councils – Volunteer groups that support school programming, organize events, fundraise for equipment and field trips, and foster family engagement.
- School Community Partnerships – Joint use of school fields and gyms for community sports, meetings, and programs in coordination with the community league.

These councils help link neighbourhood families with educational and youth focused opportunities.



Faith-Based and Cultural Organizations

Churches, temples, and other faith communities in and around Parkview often function as informal community hubs by:

- Hosting social gatherings, youth groups, seniors' luncheons, and holiday events.
- Providing support services such as food hampers, referral to social agencies, and newcomer assistance.
- Offering space for community meetings, support groups, and cultural activities.

While serving specific congregations, many of these organizations are open to broader neighbourhood participation.

PROJECT CONSIDERATIONS

The Parkview Community League is a key partner for sharing project information, gathering feedback, and coordinating use of community hall and grounds. Local sports and recreation associations, as well as school councils, rely on safe access to fields, rinks, and routes to schools; phasing, detours, and temporary closures should be planned in collaboration with these groups to minimize disruption to programs and events. Faith-based and cultural organizations, along with social, service, and advocacy groups, can help reach diverse residents, including families, seniors, and newcomers, and can support targeted communication about construction impacts, safety, and opportunities for input. Using existing communication channels (league website/social media, school newsletters, and City resource pages) will support transparent, timely updates and encourage ongoing community participation throughout the Neighbourhood Renewal process.



Community-activated roadway green on 92 Avenue

GBA+ Considerations

Gender-Based Analysis Plus (GBA+) is a tool used to understand how different people experience policies, programs, and spaces. While it considers gender, the “plus” recognizes that people’s experiences are also shaped by age, income, race, culture, language, disability, education, family status, and how these factors intersect.

In the project area, applying GBA+ means recognizing that all residents do not use streets, sidewalks, parks, schools, and transit in the same way. Design choices that work well for one group may create barriers for others. This analysis provides an equity lens on the transportation and public realm so that neighbourhood designs can better serve everyone who lives, works, studies, or travels through the neighbourhood. It is intended as a high-level overview, not an exhaustive social assessment.

Parkview is an established, primarily residential, west Edmonton neighbourhood characterized by:

- Higher than average household incomes and homeownership
- Predominantly single-detached housing on larger lots
- Older housing stock (many homes from the 1950s–1970s) with some infill
- Proximity to the river valley and access to green space
- Lower population density than many city neighbourhoods

These features shape who lives there, who can afford to live there, and how different groups experience the area. The focus is: who benefits, who might be unintentionally disadvantaged, and what to consider in Neighbourhood Renewal planning, engagement, design, and construction.

Reviewing current demographic data helps inform how GBA+ can be applied to better serve residents today. However, as Neighbourhood Renewal projects are designed to last 30–50 years, it is equally important to proactively advance equity through design, guided

by policy and best practices. The following themes and considerations should inform the project.

Gender

Women are more likely than men to use transit and walk, to make multiple stops on their travels (trip chaining), to travel during off-peak times, and to take short trips to serve others. Across all transportation modes, women and non-binary people report feeling less safe and are more likely to experience harassment. Women also face higher rates of transportation poverty, are under-represented in bike riding, and may encounter barriers to feeling comfortable biking. In Edmonton, women are more likely to have safety concerns when accessing public transit and to face childcare-related barriers that limit their participation in public engagement activities.

PROJECT CONSIDERATIONS

Consider elements such as:

- *Safe, connected routes for transit, walking, and biking*
- *Locations of possible destinations for “trip-chaining” such as private daycare facilities in the project area*

Sexual Orientation

LGBTQ+ residents may have different experiences of safety and comfort in public spaces, which can influence how and when they choose to use streets, pathways, transit, and parks. Even in higher-income neighbourhoods, concerns related to harassment, visibility, and social exclusion can affect travel choices, particularly for those who rely on walking, biking, or transit. Public spaces should not only be safe, but also feel welcoming and inclusive. Design elements such as clear sightlines, lighting, comfortable gathering areas, and inclusive signage can help foster a sense of belonging for people of diverse sexual orientations and gender identities. Women and gender-diverse LGBTQ+

individuals may be especially sensitive to perceived safety when using transit or active transportation routes, and thoughtful design can help reduce spaces perceived as unwelcoming or uncomfortable.

PROJECT CONSIDERATIONS

Consider elements such as:

- CPTED (Crime Prevention Through Environmental Design) principles like good lighting, clear sightlines, and multiple route options to ravines and public spaces to promote safety
- Where possible, reinforce belonging through signage and/or creative elements in infrastructure design

Education

Access to equitable transportation options affects people's ability to pursue higher education opportunities. Individuals with more education are typically better able to access and understand project details and public engagement activities because they often have higher levels of system awareness and literacy. As a result, they are also more likely to see their feedback reflected in plans and designs, since they can more easily navigate engagement processes and understand project language.

PROJECT CONSIDERATIONS

Consider elements such as:

- Use simple, clear language
- Use many communications tools to share information about the project and engagement opportunities

Religion

People may have unique travel needs on religious and cultural days.

PROJECT CONSIDERATIONS

There are no religious buildings or churches identified within project boundary, but considerations should be made when holding public engagement events, to not conflict with religious holidays that may limit attendance of certain groups.

Languages

Language can reflect bias in public communication, which creates barriers to understanding and limits awareness of different perspectives. To reduce these barriers, language should be objective, clear, and concise. This is especially important because people who do not speak or read English may struggle to access project information.

PROJECT CONSIDERATIONS

Use simple language, visuals, and universal symbols on wayfinding and project materials.



Ethnicity

Ethnic minorities are under-represented in biking communities and may face barriers to biking. Indigenous people and Visible Minorities are disproportionately ticketed along roadways and on transit by law enforcement. Newcomer and immigrant populations are also less likely to have access to a car and may experience compounded barriers related to language and bias or prejudice.

PROJECT CONSIDERATIONS

Improve public spaces where possible (e.g. seating, flexible plaza spaces that may support informal gathering or cultural events).

Employment

Access to equitable transportation options expands people's employment and economic opportunities. In line with this, the City Plan envisions nodes and corridors being developed to support 50% of all employment in Edmonton, linking jobs more closely with accessible transportation.

PROJECT CONSIDERATIONS

Consider elements such as:

- *Schedule engagement events at varying times (evenings, weekends, etc.) to offer options to those who may have scheduling challenges*
- *Provide flexible input options that can be completed on a participant's schedule*

Housing

Housing type does not necessarily correlate with travel choice. Although residents in lower-income housing tend to have higher transit usage, mode choice is shaped by a range of factors. Providing integrated non-vehicle transportation options can improve affordability and accessibility for meeting daily needs. Even in established neighbourhoods with high rates of vehicle ownership, barriers may persist for individuals with lower incomes, medical needs, or care-giving requirements.

PROJECT CONSIDERATIONS

Provide access to active transportation routes, consider street parking (including accessible parking) needs for caregivers, home care workers, and service workers – more than just for homeowners.

Climate & Environment

Urban design that reduces pollution and provides shelter from heat and severe weather can particularly benefit marginalized groups who rely on walking, rolling, and transit. Although low-income commuters typically contribute far less to air pollution and greenhouse gas emissions, they are more likely to experience the negative health and environmental impacts.

PROJECT CONSIDERATIONS

Consider elements such as:

- *Design options to increase active travel*
- *Low Impact Development (LID)*
- *Preservation of green spaces and adding trees where possible*

Disability

Many people with disabilities cannot drive or afford a vehicle and therefore rely on universally designed features in non-vehicle modes of transportation. However, hazards and directional cues that are easy for sighted people to recognize can be confusing or dangerous for people with visual impairments, underscoring the need for truly accessible design.

PROJECT CONSIDERATIONS

Consider elements such as:

- *Incorporate Access Design Guide into project components*
- *Ensure accessible routes and crossings (e.g. wide sidewalks, curb ramps, and gentle slopes)*
- *Use clear, inclusive design cues (e.g. good lighting, wayfinding, and visual contrast)*
- *Review opportunities for accessible seating and reduced barriers for access to green spaces*

Age

The ability to age in place is dependent on access to necessary services, like healthcare and grocery stores, access to transit and other modes of transportation, and opportunities for social connection. This includes providing safe routes within the neighbourhood and connections to the broader transportation network for walking and rolling. Seniors may prefer different engagement approaches than other groups, and Parkview East's age, income, and local context suggest there may be "snowbirds" whose seasonal absences affect participation. In addition, parents with school-aged children often face time constraints, especially in summer and at the start of the school year, which limits their availability for public engagement events.

Designing walking, rolling, and biking facilities for all ages and abilities can encourage broader use, including by children and older adults who are considered vulnerable road users in roadway design. Design should consider likely destinations by these groups and provide accessible routes.

PROJECT CONSIDERATIONS

Consider elements such as:

- *Identify any walking, biking, or transit routes connecting children and youth to nearby schools*
- *Identify any destinations for children and seniors, and exploring opportunities to improve comfort and safety along routes (e.g. bench seating at frequent intervals for seniors near ravine)*
- *Hold public engagement events at varying times of year to provide opportunities for all demographics to attend*



Income

Income can influence how residents travel, access public spaces, and participate in neighbourhood planning. While many households may have access to private vehicles, residents with lower or fixed incomes including seniors, students, and people with disabilities may rely more on walking, biking, or transit and can be more affected by gaps in pedestrian or biking infrastructure. The costs associated with neighbourhood improvements may also have differing impacts across income levels. In addition, work schedules, caregiving responsibilities, and access to technology can affect the ability of some residents to participate in engagement activities, highlighting the importance of offering flexible and accessible engagement options.

PROJECT CONSIDERATIONS

Consider elements such as:

- *Schedule engagement events at varying times (evenings, weekends, etc.) to offer options to those who may have scheduling challenges*
- *Provide flexible input options that can be completed on a participant's schedule*
- *Collect and crosstab demographic information on household income in the surveys to allow the project team to understand how income may influence opinion throughout the project*



Policy Context

The Parkview East Neighbourhood Renewal project is guided by the City of Edmonton's strategic planning framework, led by ConnectEdmonton, which establishes the long-term vision and priorities for the city. This strategic plan is supported by a coordinated set of policies, standards, and guidelines that collectively inform how neighbourhoods are planned, designed, and renewed. These documents provide direction on mobility, urban design, climate resilience, equity, open space, and public realm improvements. Together, they ensure that Neighbourhood Renewal projects align with citywide objectives while responding to local context, existing infrastructure, and community needs.

Relevant Plans

ConnectEdmonton

ConnectEdmonton is the City's strategic plan for 2019–2028. The policy is governed by one unifying principle, Connected, which guides all aspects of the plan. The following four strategic goals are the areas which require transformational change to achieve the long-term vision for Edmonton in 2050.

Healthy City

The Healthy City goal focuses on fostering community and personal wellness by ensuring all Edmontonians have equitable access to the conditions that support a good quality of life. A healthy city is one where people feel safe, empowered, included, and connected to those around them. This goal emphasizes reducing disparities in wellbeing, strengthening community relationships, and enhancing opportunities for residents to thrive. By creating supportive environments and advancing equity, Edmonton works toward a future where every resident can live their best life.

Regional Prosperity

The Regional Prosperity goal positions Edmonton as a key driver of economic growth, innovation, and competitiveness within the metro region. By collaborating with regional partners, Edmonton aims to cultivate diverse economic opportunities, attract global investment, and support homegrown talent and businesses. This goal recognizes the interconnected nature of the region and seeks to enhance shared prosperity by strengthening economic resilience, improving productivity, and building relevance on national and international stages.

Urban Places

The Urban Places goal envisions Edmonton's neighbourhoods becoming more vibrant, denser, and better connected, offering diverse housing options, attractive destinations, and thriving local businesses. Urban places are defined by high-quality design, walkability, accessible amenities, and strong transportation choices that collectively create engaging and livable communities. This goal highlights the importance of integrating land use, mobility, and development to support sustainable growth and create urban experiences that draw people to live, work, shop, and play in Edmonton.

Climate Resilience

The Climate Resilience goal commits Edmonton to transitioning toward a low-carbon future while preparing for and mitigating the impacts of climate change. It emphasizes protecting people, infrastructure, and ecosystems they inhabit through improved adaptation, cleaner energy use, reduced greenhouse gas emissions, and high standards for air and water quality. Achieving this goal requires coordinated action to safeguard environmental health today while ensuring future generations inherit a city capable of withstanding and responding to a changing climate.

The City Plan

The City Plan sets out Edmonton's long-term direction for how the city will grow, move, and evolve as it reaches a population of two million. It integrates land use, mobility, open space, and climate considerations so road construction, transit investment, and urban design work together to support compact communities and all-season livability. The plan prioritizes active transportation and connected public spaces to create a city that is more vibrant, accessible, and resilient in winter and summer alike. The plan outlines five Big City Moves which then define city building priorities to deliberately shape the future of the city.

Greener as We Grow

Planting two million new urban trees.

Project Consideration: Additional trees will be planted in boulevards and open spaces where possible.

A Rebuildable City

600,000 additional residents will be welcomed into the redeveloping area.

Project Consideration: Renewed surface and underground infrastructure will support generational turnover as younger families move into the neighbourhood.

A Community of Communities

50% of trips are made by transit and active transportation.

Project Consideration: Improvements to active transportation routes and transit connectivity will support choices other than vehicle-based transportation.

Inclusive and Compassionate

Nobody is in core housing need and less than 35% of average household expenditures are spent on housing and transportation.

Project Consideration: Adding active transportation missing links and improving intra-neighbourhood connectivity can promote less costly transportation choices.

Catalyze and Converge

Hold 70% of total regional employment in Edmonton.

Project Consideration: Refreshed infrastructure will solidify the project area as a central choice for local workers to live for decades to come.

District Policy + Jasper Place District Plan

The District Policy sets City-wide policy to enact the City Plan. The Jasper Place District Plan dictates how and where the District Policy will be applied. Parkview is within the Jasper Place District.

The Jasper Place District Plan serves as a guiding framework for coordinated redevelopment and reinvestment across the Jasper Place area, which includes several mature neighbourhoods such as Parkview, Meadowlark Park, West Jasper Place, and Sherwood.

The plan's intent is to support the evolution of established communities by balancing reinvestment in aging infrastructure, housing, and public spaces with the preservation of valued neighbourhood character. It also seeks to ensure that future growth occurs in a manner consistent with Edmonton's City Plan.

Key Policy Directions and Themes

- **Neighbourhood Identity & Character:** Reinforce the established residential form and tree-lined streets typical of mature neighbourhoods, while allowing for gentle infill and renewal that respects existing scale and massing.
- **Connectivity & Streetscape:** Improve the pedestrian and biking environment, emphasizing safe, accessible, and attractive streetscapes to strengthen connections to key corridors (e.g. 142 Street, 87 Avenue) and local destinations.
- **Public Realm Enhancement:** Upgrade parks, open spaces, and community facilities to promote vibrancy and year-round use.
- **Commercial Revitalization:** Support neighbourhood-scale commercial redevelopment along primary corridors, maintaining compatibility with adjacent low-density areas.
- **Housing Diversity:** Encourage a broader mix of housing types (e.g. duplexes, small-scale infill) within existing form guidelines, accommodating aging-in-place and demographic change.
- **Sustainability:** Promote green infrastructure, improved transit accessibility, and environmentally responsible design approaches.

Relevance to Parkview East

The Jasper Place District Plan provides policy context and design direction for understanding the character and potential evolution of Parkview's eastern portion. It will inform the Urban Design Analysis in several ways:

Connectivity & Corridors:

Its directives on improved pedestrian and biking linkages support identifying opportunities for enhanced east-west and north-south mobility, particularly connections to 142 Street and community amenities.

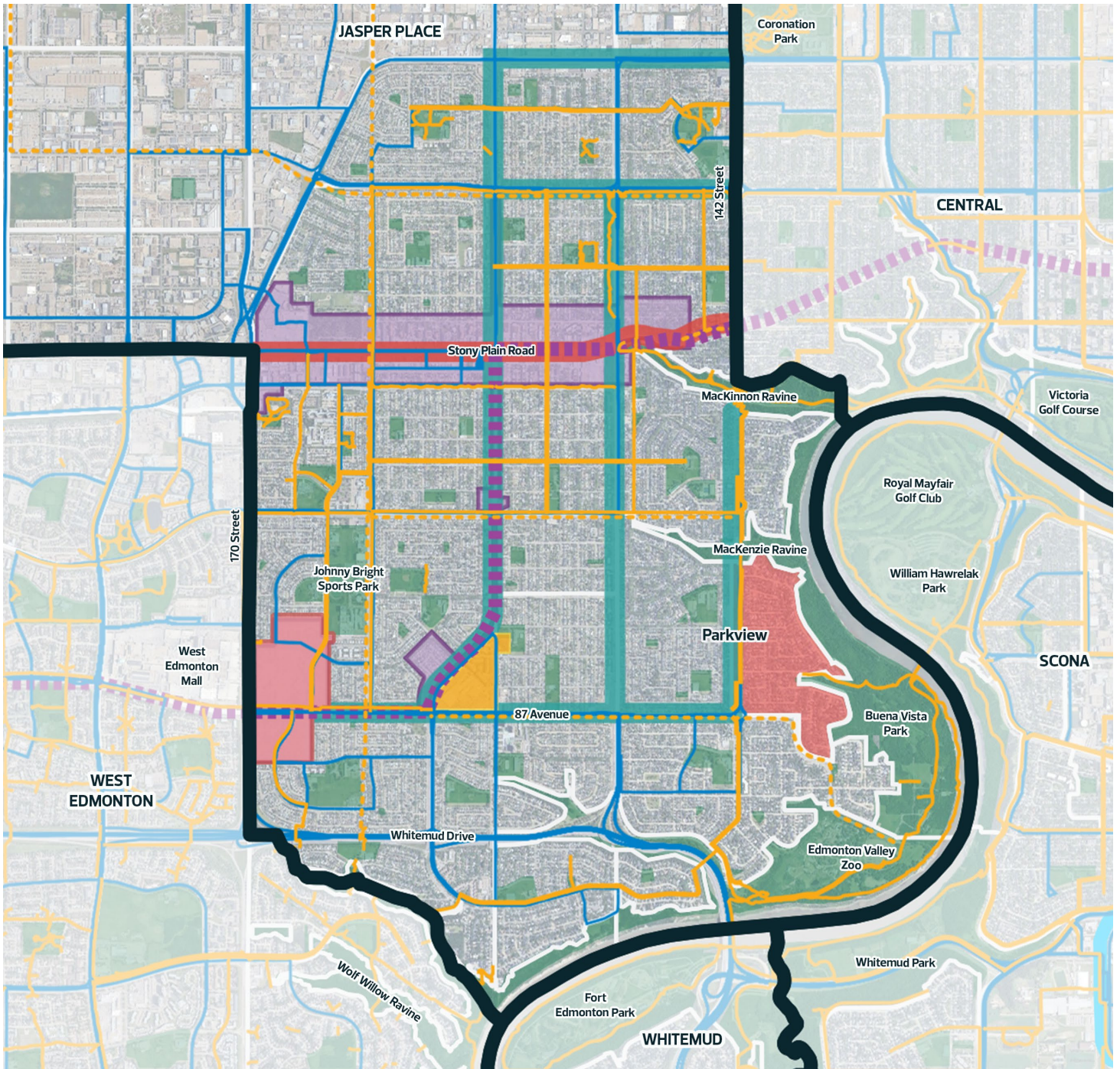
Public Realm Framework:

The plan supports enhancements to sidewalks, lighting, and green spaces. The analysis should evaluate how renewal investments can improve comfort, safety, and aesthetic quality in public areas.

Sustainability & Urban Forest:

The district plan's sustainability goals will help frame strategies for integrating low-impact development features and preserving the mature tree canopy.

Project Considerations: The Jasper Place District Plan provides a high-level vision that encourages design improvements that promote connectivity, livability, and long-term sustainability. The Urban Design Analysis for Parkview East will translate these planning goals into site- and streetscape-level observations and recommendations to shape the neighbourhood's physical renewal priorities.



Map 6 – District Plan Considerations

Map Not to Scale



Map Legend

- Project Area
- Primary Corridor
- Secondary Corridor
- Bike Route
- District Boundary
- Existing LRT
- Future Bike Route
- Parks and Open Space
- Major Node
- Future LRT
- Bus Route
- District Node
- Priority Growth Area

Relevant Policies

Climate Resilient Edmonton: Adaptation Strategy and Action Plan

The Climate Resilient Edmonton: Adaptation Strategy and Action Plan outlines how Edmonton will prepare for and respond to the impacts of a changing climate by strengthening the resilience of its infrastructure, natural systems, and communities. The plan emphasizes that built infrastructure will face increasing stresses from heat, freeze-thaw cycles, flooding, and extreme weather. This will require more durable materials, thoughtful design standards, and proactive asset management. It highlights the importance of integrating climate adaptation into urban design which will ensure streetscapes, public spaces, and redevelopment areas support cooling, manage stormwater, and remain safe and functional year-round, particularly within Edmonton's winter season.

The strategy underscores how enhancing pedestrian and biking infrastructure, improving connectivity, and designing comfortable four-season public spaces can boost climate resilience while supporting vibrant, accessible neighbourhoods. The plan positions climate adaptation as a core factor in shaping Edmonton's future mobility networks and urban environments, ensuring they remain livable, safe, and attractive as conditions continue to change.

A Greener Edmonton: A Vision for Restorative Actions on our Urban Landscape

A Greener Edmonton: A Vision for Restorative Actions on our Urban Landscape outlines the approach to restoring and enhancing public landscapes by strengthening natural systems and improving the ecological function of parks, open spaces, and corridors. The plan guides how restorative activities are chosen and implemented so that naturalization and tree planting become integral to the way Edmonton designs and renews neighbourhoods and urban spaces. Its direction supports greener, more resilient streets and pathways that provide shade, manage stormwater, and improve comfort for people walking or biking throughout the year. By expanding the urban forest and creating better links between natural areas, the plan enhances pedestrian connectivity and contributes to more vibrant and welcoming public spaces across Edmonton.

Breathe: Edmonton's Green Network Strategy

Breathe: Edmonton's Green Network Strategy guides how the city's parks, open spaces, and green corridors are planned and improved so they better support everyday life, environmental resilience, and year-round mobility. It highlights how urban design can integrate greener streets and create comfortable active transportation routes in all seasons. By linking open spaces and enhancing the public realm, the strategy helps create a more vibrant, accessible, and enjoyable city for residents and visitors.



Desire lines from people walking along Valleyview Drive, and service vehicle tracks from garbage pickup

Open Space Policy

The Open Space Policy guides how Edmonton's parks, natural areas, and pedestrian-oriented spaces are planned and designed as a connected network that supports ecological health and community wellbeing. It links open space planning with urban design to strengthen active transportation, winter comfort, and pedestrian connectivity.

This policy will be applied to the river valley top of bank area and internal roadway greens. The renewal will consider the guidance to provide enhanced walkways, seating, lighting and barrier free access to allow access to these amenities for all ages and abilities.

Urban Forest Management Plan

The Urban Forest Management Plan sets the direction for protecting, growing, and managing Edmonton's public and private trees as essential urban infrastructure. It guides how urban design projects incorporate tree preservation and planting to improve comfort, shade, wind protection, and the overall experience of walking, rolling, and biking through the city. By strengthening canopy coverage along streets and pathways, the plan enhances pedestrian connectivity and contributes to safer, more attractive, and more vibrant public spaces.

Safe Mobility Strategy

The Safe Mobility Strategy sets Edmonton's direction for achieving Vision Zero by redesigning streets and transportation networks so people can move safely and comfortably in all seasons. It guides how the design of Neighbourhood Renewal projects should incorporate safer crossings, lower speeds, and all-season infrastructure that supports walking, biking, rolling, and transit. By focusing on high-injury locations and winter mobility challenges, the strategy aims to strengthen pedestrian connectivity and create streets that feel more welcoming to all. It positions safety as a foundation for building a more accessible and livable city.



142 Street service road



City policies encourage safe infrastructure for people of all ages and abilities

Relevant Standards + Guidelines

Complete Streets Design and Construction Standards Volume 2 (Roadways)

The Complete Streets Design and Construction Standards govern how road right-of-way infrastructure should be designed and built. The project team will incorporate these principles into the Parkview East design, and any deviations will be supported through standard industry best practices and precedents from other jurisdictions.

Travelled Way

Street cross-sections for Neighbourhood Renewal will be created with guidance from the standards. The Parkview East project area consists entirely of local streets. The project team will look for opportunities to adjust vehicle space to create streets suited for multimodal use. This will include adding and/or widening sidewalks and pathways, and street designs that better accommodate people using active transportation (including walking, rolling, and biking), while maintaining vehicular access and parking.

Traffic Calming

The guidelines identify potential traffic calming features to manage circulation within neighbourhoods. Their aim is to reduce vehicular speeds, discourage through traffic, minimize conflicts between street users, and improve the neighbourhood environment. Potential measures are categorized into the categories of vertical or horizontal deflection, access restriction, intersection treatment, and signage. The project team will evaluate each measure identified for implementation in Parkview East by considering the potential benefits and drawbacks of feasible traffic calming features.

Climate Resilience

Neighbourhood Renewal projects must be designed for the climate conditions of today as well as the future, which includes increasing temperatures and precipitation. Project designs should include elements to reduce the urban heat island effect, increase resilience to urban flooding events, and maintain healthy ecosystems with ecological planning along transportation routes. Specific actions include reducing hard surface coverage, densifying the tree canopy, and increasing active transportation connections to promote usage.

Low Impact Development (LID)

The standards note that renewed roadways should be designed where feasible to include LID. This promotes flood resistance along transportation corridors and are less land intensive than some other stormwater management systems. Key areas for LID use in Parkview East will be adjacent to roadways or in parks and open spaces. The City has identified that only absorbent landscaping will be considered for LID facilities within Parkview East. LID facilities must also be set back 50m from the top of bank of the river valley and ravines.

Winter Design

As a winter city, the impact of winter should be considered at the beginning of the design stage. Thought will be given to snow clearing and storage operations, maximizing sunlight and reducing wind exposure, and selecting climate appropriate landscaping and salt resistant plant species.



The Bike Plan and Implementation Guide

The Bike Plan and its corresponding implementation guide outline how Edmonton will deliver a connected, safe, all-season biking network that enhances everyday mobility and supports more vibrant streets. Together, they guide how bike infrastructure is integrated into neighbourhood reconstruction, ensuring renewed streets include protected facilities, better crossings, and clearer links to key destinations. The implementation guide reinforces this by prioritizing district connector routes, aligning projects with renewal programs, and supporting all-season maintenance so routes function reliably in winter. These directions help shape urban design that strengthens biking connectivity, improves safety, and adds activity and vibrancy to neighbourhoods as they evolve.

Access Management Guidelines

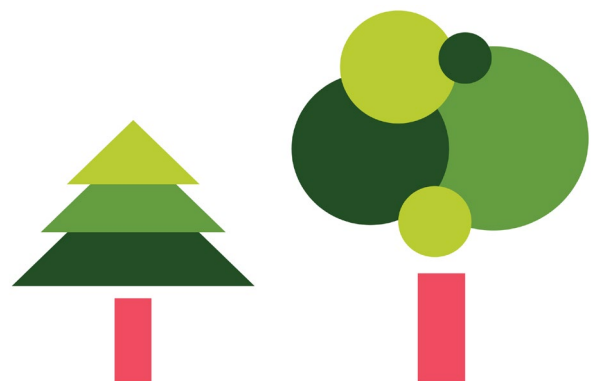
The Access Management Guidelines provide direction for how site accesses are planned and designed so Edmonton's road network functions safely and efficiently as redevelopment and new construction occur. The guidelines ensure that driveway and access placement supports urban design goals, protects pedestrian and biking routes, and aligns with road classifications and modal priorities so active transportation and transit remain reliable. By requiring careful spacing, sight lines, and crossing designs the guidelines help create safer and more comfortable conditions for walking and rolling. They support more vibrant and connected streets by reducing conflicts and reinforcing a streetscape that prioritizes people while allowing the movement of vehicles.

Access Design Guide

The Access Design Guide considers the needs of all users regardless of age and ability for accessing public infrastructure. In the context of Neighbourhood Renewal, the focus is on creating barrier-free connections from within the neighbourhood to open spaces, transit facilities, employment, and commercial areas.

Winter Design Guidelines

The Winter Design Guidelines provide a framework for shaping streets, buildings, and open spaces so they function comfortably and safely in Edmonton's winter climate. The guidelines influence how neighbourhood designs should incorporate sun exposure, wind protection, lighting, and materials to create inviting streets that support walking and rolling, biking, and transit – even in cold and dark conditions. By improving pedestrian connections, enhancing crossings, seating, and lighting, the guidelines strengthen active transportation and year-round mobility. Overall, they help create more vibrant public spaces and neighbourhoods by turning winter into an asset and ensuring the city remains lively and accessible in all seasons.



North Saskatchewan River Valley and Ravine System Protection Overlay (2.260 RVO)

The North Saskatchewan River Valley and Ravine System Protection Overlay dictates the development setback distance from the river valley and ravine system to mitigate risks associated with top-of-bank landslides, erosions, and other hazards. The project team will consider this restriction for any top-of-bank features.

North Saskatchewan River Valley Area Redevelopment Plan

The North Saskatchewan River Valley ARP outlines the land use pattern in the Edmonton-area river valley and outlines the goals of preserving the ecological systems within it and providing public access to this key asset. The Plan states that the river valley area adjacent to Parkview East should remain entirely zoned as open space. Within the scope of this project, existing access to the river valley will be maintained, and connections to these access points within the neighbourhood should be enhanced through the addition of missing links and wayfinding.

Ribbon of Green Strategic Plan

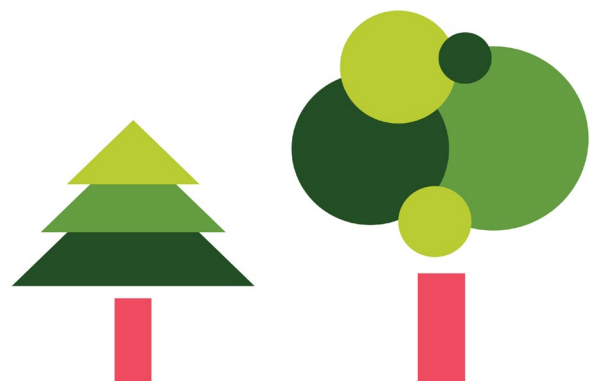
The Ribbon of Green (ROG) Strategic Plan ensures that the river valley is protected and continues to be a vibrant open space network as the city expands. It provides policy direction to guide the management of the river valley and ravine system in Edmonton. Parkview East borders both the North Saskatchewan West and Central ROG spatial areas with specific visions and program guidance for each.

North Saskatchewan West Reach

This west reach is defined by the large intact forested habitats and steep cliff faces. In this project connections and accessibility to the surrounding trail network leading to the Edmonton Valley Zoo/Sir Wilfrid Laurier Park and Buena Vista Park Amenity Nodes can be strengthened.

North Saskatchewan Central Reach

The central reach is a developed and well-used area of the river valley. It has thick forested areas and steep slopes that face risk of erosion. Similar to the west reach, connections to existing trail networks can be improved during the Parkview East project.



Neighbourhood Analysis

Places + Destinations

Parkview East is a primarily residential neighbourhood with strong connections to the North Saskatchewan River Valley. Notable destinations include the vegetated top of bank area with an off-leash dog park, panoramic river valley views, access points to the river valley trail system; and small roadway greens distributed throughout the neighbourhood. The area also benefits from proximity to the commercial node near 142 Street and 91 Avenue, which serves as an important daily destination for residents and helps draw non-residents into the neighbourhood.

Key aspects of these places and destinations are evaluated through Crime Prevention through Environmental Design (CPTED) principles, winter city, and equity lenses, with attention to safety, visibility, comfort, and year-round accessibility. The analysis identifies several missing or substandard elements that negatively affect the public realm, including limited accessible trail connections, a lack of accessible seating and waste receptacles, limited lighting along the top of bank, and missing sidewalks in front of some residential homes. These factors will inform Neighbourhood Renewal decisions aimed at improving connectivity, accessibility, and overall public realm quality.

Land Use + Built Form

Land Ownership

Public Lands

City-owned land within Parkview East is limited to road right-of-ways, public utility lots, and the North Saskatchewan River Valley.



Parkview East has numerous infill developments like these along 140 Street

Private Lands

Private land within Parkview East consists entirely of low-density, single-detached residential parcels. Over the past several decades, the neighbourhood has experienced a steady influx of large-scale single-detached infill developments. Several of these infill projects have included garage suites, a trend that remains prevalent.

Built Form + Character

Parkview is a mature, low-density neighbourhood situated along the edge of the North Saskatchewan River Valley, characterized by a strong mid-century development style. Its urban form features a modified grid and wide roads with predominantly curbside sidewalks, and residential lots composed largely of single-detached homes built in the 1950s and 1960s. While the original housing stock consisted mainly of modest bungalows and split-level homes, the neighbourhood has experienced significant reinvestment in recent decades through large-scale single-detached infill development. This trend is especially evident in Parkview East, where larger lots and proximity to the river valley have contributed to a more estate-like character.

One home constructed during the height of Parkview's original development was designed by notable Edmonton architect David Lieberman and served as his personal residence. Lieberman's work is considered a leading example of the city's modernist architecture. This legacy is reflected in the distinct style of his former residence.



Development in the project area includes older homes (Valleyview Crescent)

Commercial development in Parkview is minimal, limited to small, community-scaled nodes along 142 Street that offer local services and reinforce the neighbourhood's residential identity. Within Parkview East, there are no commercial properties, however there are several home-based businesses, which include health and wellness studios, tattoo studios, and child-care businesses. These operations are generally low intensity and primarily appointment-based, resulting in minimal traffic and limited impact on the surrounding residential context.

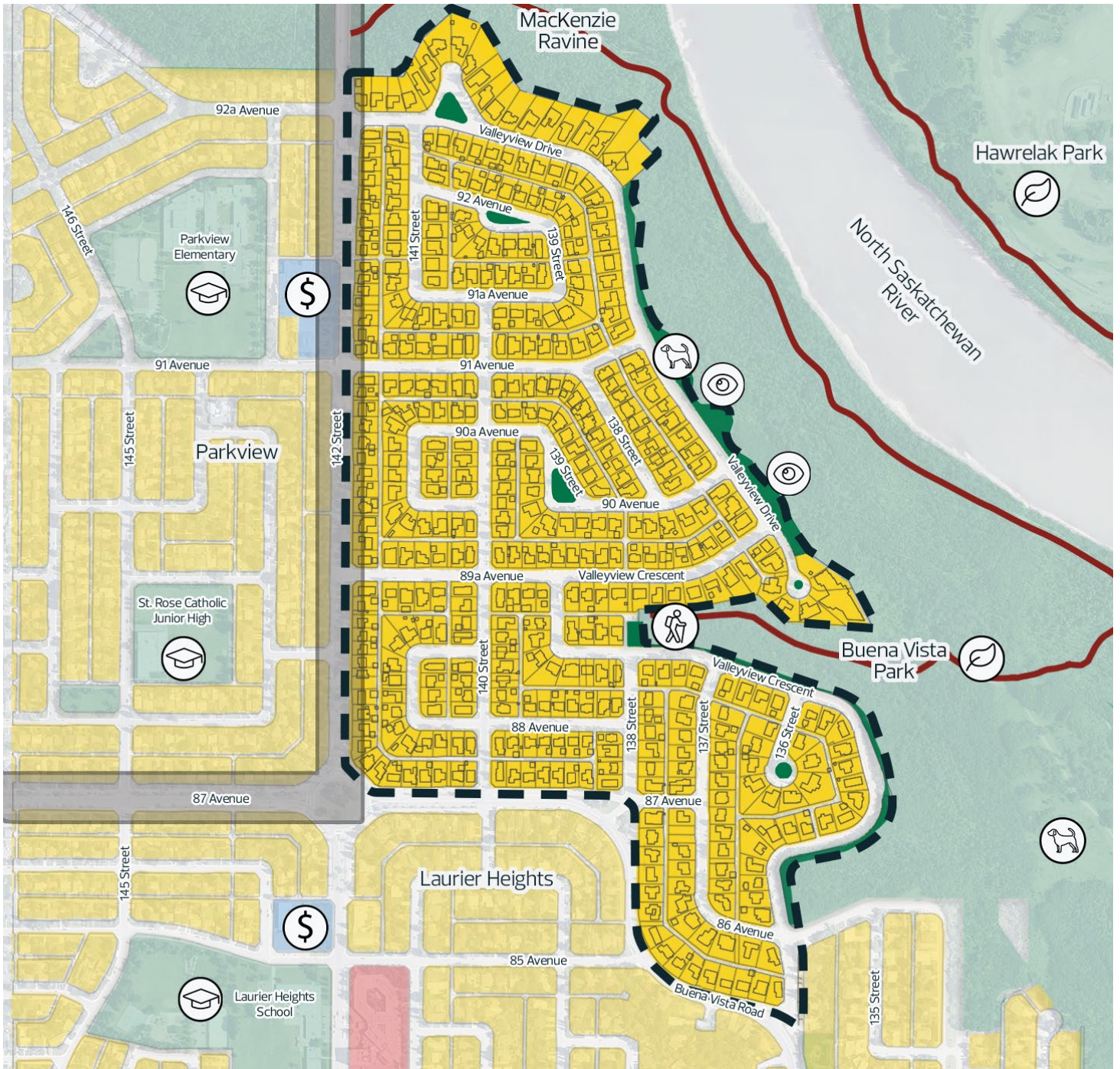
The City Plan identifies 142 Street as a secondary corridor. Secondary corridors are vibrant areas, which are residential in nature, that contain commercial clusters and serve as a destination for surrounding communities. The identity of 142 Street will be reinforced with ongoing investment to increase density and growth along the corridor.



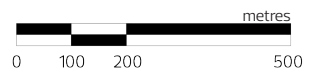
Development in the project area includes new infill homes (92 Avenue)
















Development in the project area includes renovated homes (88 Avenue)



Map 7 – Destinations + Built Form + Character



Map Legend

- | | | | | | |
|-------------------------------------------------------------------------------------|-------------------|-------------------------------------------------------------------------------------|-----------------------------|---------------------------------------------------------------------------------------|---------------------|
|  | Project Area |  | Granular Pathway |  | River Valley Access |
|  | Dog Park |  | Roadway Greens | | |
|  | Destination Park |  | Residential | | |
|  | School |  | Commercial | | |
|  | Point of Interest |  | Institutional | | |
|  | Commercial |  | Secondary District Corridor | | |

Green Infrastructure Network

Although Parkview East has a strong connection to the North Saskatchewan River Valley, including direct access to the top of bank with an off-leash area and scenic lookouts, open green space within the interior of the project area is limited. Four roadway greens which serve as small pocket parks are located within this area: one along the north side of Valleyview Drive; another along 92 Avenue near 139 Street; a third along 139 Street between 90 Avenue and 90A Avenue; and a fourth in the cul-de-sac on 136 Street. Beyond these community used roadway greens, a small, densely landscaped traffic island located in the Valleyview Point cul-de-sac also contributes to the green infrastructure of Parkview East. A small parcel near Valleyview Crescent is an entrance to Buena Vista Park and provides minor supplementary open space. Formal park space in the Parkview neighbourhood is located west of 142 Street around Parkview Elementary School, Community League, and St. Rose Junior High School. These park spaces are within walking distance from only a small portion of the Parkview East area. Despite the limited green space within the project area, Parkview East benefits from strong access to the City's broader green infrastructure network, most notably through Buena Vista Park.

In neighbourhoods such as Parkview East, where residential lots are generally larger and often include well-established, high-quality private landscaping, the role and use of public parks and open space can differ from areas with limited private outdoor space. Private yards may accommodate a range of everyday recreational and social activities, which can reduce reliance on formal park spaces for routine use. As a result, public open spaces in these contexts function as destinations, supporting passive recreation, access to natural features, and connections to the broader open space network, rather than as primary settings for daily outdoor activity.

All trees within the public right-of-way are considered City-owned assets and are incorporated into the City's Urban Forestry inventory. As such, these trees are assigned an asset value and are subject to the City of Edmonton's standards for maintenance, management, and protection.



Mountain biking is one of many activities people enjoy in the North Saskatchewan River Valley

North Saskatchewan River Valley Park Network

Parkview residents benefit from direct access to the North Saskatchewan River Valley, one of Edmonton's most significant natural amenities and park systems. Positioned along the valley's edge, the neighbourhood offers several scenic viewpoints and a trail connection linking residents to the broader river valley park trail network. This proximity creates daily opportunities for walking and rolling, biking, and enjoying natural views, reinforcing Parkview's strong connection to the city's larger green network.

The image to the right illustrates lands that are within the North Saskatchewan River Valley area.



Edmonton's river valley park system bounds the east side of the project area

Roadway Greens

Roadway greens are green spaces located within public road right-of-way, typically found as traffic islands, medians, or boulevards. In a residential context they can function as usable open space for residents to enjoy.

Unnamed Roadway Green - Valleyview Drive

The existing landscaping is grass with mature trees and a single bench. There are no sidewalks or curb ramps on or leading to the space which makes accessibility a challenge.



Valleyview Drive Roadway Green

Unnamed Roadway Green - 92 Avenue + 139 Street

This Roadway Green functions as a small pocket park for the neighbourhood. It is primarily composed of mature trees, with some grassy areas at the west and east ends. The narrow shape and dense tree coverage limit potential use; however, residents have installed a basketball net for recreational activities on the surrounding roadway. There are no sidewalks or curb ramps on or leading to the space, which makes accessibility challenging.



92 Avenue Roadway Green

Unnamed Roadway Green - 90 Avenue + 139 Street

This Roadway Green functions as a small pocket park for the neighbourhood. The green space is located between two road surfaces along 139 Street. It features a mix of mature trees and grassy areas, along with a bench. Residents have placed two large movable soccer nets in the open space, and there is potential for other informal recreational activities without interfering with existing use. As with other road-adjacent green spaces, the lack of sidewalks and curb ramps around and leading to the space makes access challenging.



90 Avenue Roadway Green

Unnamed Roadway Green - 136 Street Cul-de-sac

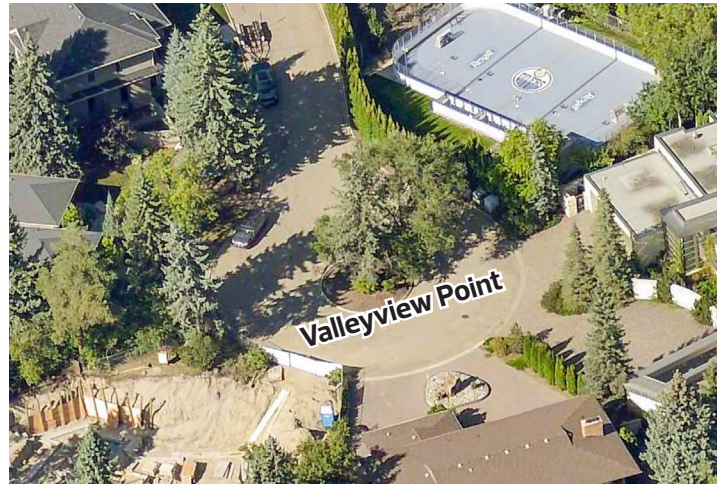
This larger median at the center of a large cul-de-sac is landscaped with large mature trees and shrubs, with no open space for use. The project team noted the presence of resident-introduced vegetation and a homemade sign naming the space as "Joey Moss Park." There may be opportunities to supplement the existing trees and plantings.



136 Street Roadway Green

Unnamed Roadway Green – Valleyview Point Cul-de-sac

This smaller median, located at the centre of a cul-de-sac, is landscaped with large, mature trees and does not provide usable open space. The project team noted the presence of numerous underground utilities running beneath the median and the narrow roadway that encircles it, which will restrict opportunities to expand or further enhance the space. It was also observed that the established trees are located in close proximity to the curb and adjacent catch basins along the perimeter of the median. This condition will require careful consideration throughout the project to ensure the retention of these trees and support their long-term viability following construction.



Valleyview Point Roadway Green

Top of Bank Open Space – Valleyview Drive

This open space is located on the east side of Valleyview Drive. There are no formal pathways, however the space does have benches and waste receptacles, and offers viewpoints of the river valley. Other than street lights along Valleyview Drive, there is no lighting within this open space. A portion of this space is designated as an off-leash dog area. There is an opportunity to provide formal pathways within the space to connect benches and gathering spaces, and create accessible connections to the rest of the neighbourhood.



Top of bank open space Valleyview Drive

Unnamed Open Space – Valleyview Crescent

This larger open space, consisting of only grass and a single crabapple tree, is on land that is partially within road right-of-way (roadway greens) and the river valley. It acts as an informal connection to the Buena Vista Park trail entrance and staging area. The space presents an opportunity to formalize this connection and enhance the area for people who use the variety of facilities in the river valley below.



Buena Vista Park trail access near Valleyview Crescent

PROJECT CONSIDERATIONS

Improve connectivity and usability of the existing roadway greens and open-space fragments through design strategies guided by the Breathe Policy, Complete Streets Design and Construction Standards, and Climate Resilient Edmonton. This includes creating safe, well-lit active mode routes, enhancing trail links to the river valley and nearby community amenities, adding seating and winter-friendly amenities, and ensuring crossings and pathways support all-season mobility for residents of all ages and abilities. Pedestrian-oriented lighting could be considered for roadway greens and top-of-bank areas to provide community spaces that are more comfortable and inviting for community members – particularly during evenings and in winter.





Map 8 – Open Space Network



Map Legend

-  Project Area
-  Roadway Greens
-  Tree (City Inventory)
-  Granular Pathway
-  Other Trail
-  Top of Bank
-  River Valley Access
-  Dog Park
-  Point of Interest
-  Destination Park

Mobility Network

The project area's mobility network is dominated by vehicle-oriented infrastructure characterized by wide streets lined with curbside concrete sidewalks and a lack of dedicated active transportation facilities for biking. There are a small number of roads that have a walkway only on one side of the roadway, most notably Valleyview Drive and portions of Valleyview Crescent, while the 142 Street service road (2-way) has no sidewalk or walkway.

There are no schools in the project area, so there are no fixed school bus routes. There are no collector roads within the project area, so there will be no ETS School Special buses. However, there may be school buses on local roads in the project area, and the routing will change over time.

There are a significant number of people who use active transportation modes to travel through the project area.

Although the 142 Street service road is the designated district bike route, the project team has observed a large number of people who bike utilizing Valleyview Drive, Valleyview Crescent, and 140 Street to traverse through the neighbourhood. This is supported by Strava data from people who record their activities on the popular fitness app.

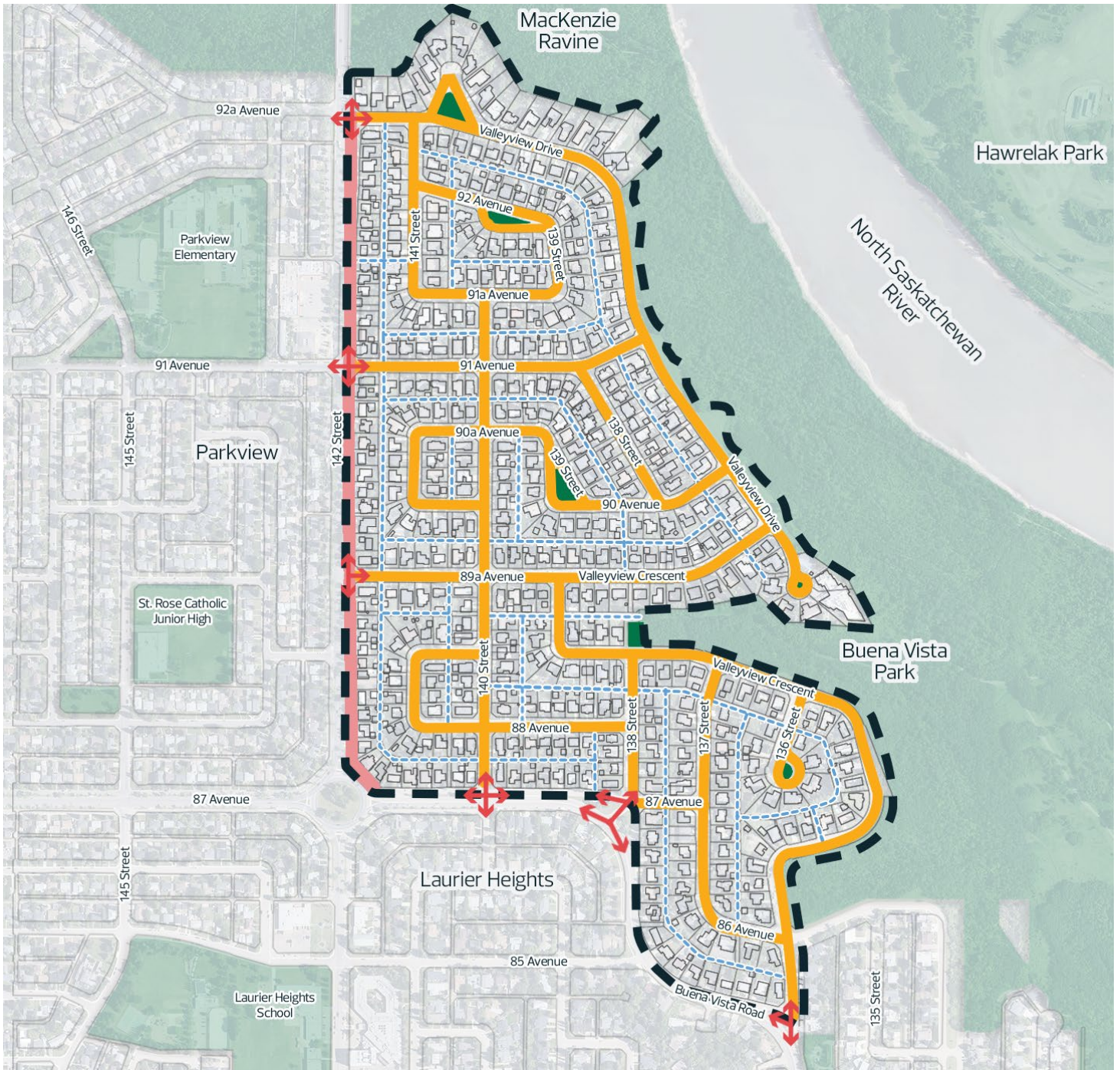
There are a significant number of people who run and walk in and through the neighbourhood for recreation or training. These are similar to the biking routes along Valleyview Drive and Valleyview Crescent, but also include east-west routes along 91 Avenue and 89A Avenue. Valleyview Drive and other roads within the project area have been a part of the course for the annual Edmonton Marathon the past several years.

Vehicular Network

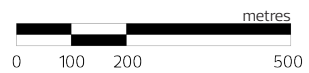
All streets within Parkview East accommodate two-way traffic and are classified as local roads. There is a service road on the east side of 142 Street within the project area. At the north end it terminates at Valleyview Drive, and at the south end it becomes a service road on the north side of Buena Vista Drive. 140 Street has two intersections with traffic calming measures in the form of adaptable curb extensions, and there are temporary speed humps on 140 Street – these measures are part of the City's Street Lab program. The city-wide default 40 km/h speed limit applies to all roads within the project area and there are no special speed limit zones present. There are very few parking restrictions within the project area which consist of short accessible parking zones in front of houses. 142 Street is an arterial roadway that bisects and services the entire Parkview neighbourhood, but the 142 Street arterial is outside of the scope of Neighbourhood Renewal.

PROJECT CONSIDERATIONS






Strengthen safety and comfort on local streets through context-sensitive traffic calming and pedestrian-focused design, aligning with the Safe Mobility Strategy and Complete Streets Design and Construction Standards. This includes evaluating existing adaptive traffic calming measures, other traffic calming measures, improving crossings near the 142 Street arterial, and ensuring all-season visibility, lighting, and comfort for all users traveling in the neighbourhood. The renewal project will have to consider existing and future walking and rolling, and biking connections to the west side of 142 Street.



Map 9 – Vehicular Network Map



Map Legend

-  Project Area
-  Local Road
-  Service Road (2-way)
-  Alley
-  Neighbourhood Access (All-Directional)
-  Roadway Greens

Public Transit Network

Parkview East is served by a single public transit corridor along 142 Street with bus stops located at 142 Street and Valleyview Drive (Stop 5198), 92A Avenue (Stop 5656), 91 Avenue (Stops 5588 and 5584), 89 Avenue (Stops 5084 and 5561), and Buena Vista Road (Stop 5052). There is also a Transit on Demand location on Buena Vista Road at 140 Street. Although no transit routes operate within the interior of the project area, most of the neighbourhood falls within the city's targeted five to ten-minute walking distance (400–600 metres) to transit service. However, the area east of 138 Street lies outside this targeted distance as do small areas within the project area where a circuitous route must be taken to walk to a transit stop on 142 Street.

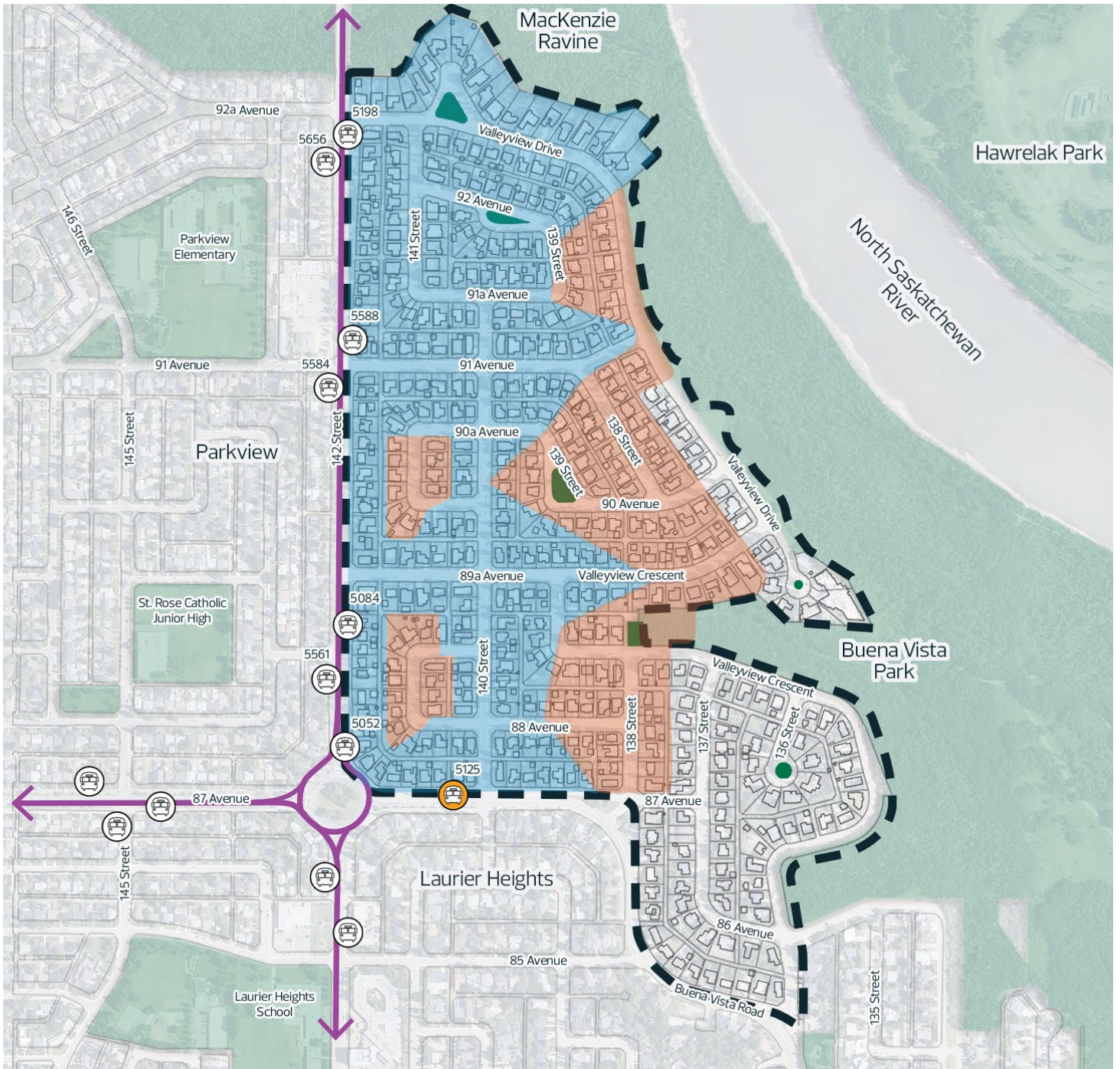
In addition to the regular bus service on 142 Street, Parkview East is also supported by DATS, with varying pickup locations and frequency based on individual rider needs. Although Edmonton Transit has current DATS ridership data, service locations can vary considerably over time. As there are no significant destinations within the project area (existing data shows stops only at residential addresses), consideration should be given to ensure that DATS buses can travel through the neighbourhood.

PROJECT CONSIDERATIONS

Create a network of connections for people walking to provide local access to transit routes along 142 Street in alignment with City policies. Investigate wayfinding opportunities for people walking. Street design should consider DATS travel throughout the neighbourhood by considering adequate space for short-term parking and loading that will allow for pick-up and drop-off.










Bus stops on 142 Street can feel exposed and be challenging to access because there are no sidewalks along 142 Street



Map 10 – Public Transit Network Map



Map Legend

-  Project Area
-  Transit Route
-  400m Walkshed
-  600m Walkshed
-  Roadway Greens
-  Bus Stop
-  On Demand Bus Stop

Active Transportation Network

Designated active transportation facilities in Parkview East are limited to narrow curbside concrete sidewalks that line the local road network. There are a few locations with missing sidewalk links, and there is no dedicated bike infrastructure or shared pathways.

Bench seating is available along the top-of-bank green space on Valleyview Drive where approximately eight benches are currently provided. There are currently no street lights in the top of bank space. Despite the neighbourhood's proximity to the North Saskatchewan River Valley and its extensive trail system, Parkview East has limited direct connections to these trails.

There is one formal trail access to the river valley located in the alley north of Valleyview Crescent near 138 Street. The gravel access has a small area for vehicle parking at the trailhead. The granular trail is used by people who walk and bike. There is also a grassed, sloped space that leads to the granular pathway. It is short and steep, and any potential improvements would likely have to consider stairs. There are also a few informal trails that connect between Parkview East and the river valley park space.

Biking within the neighbourhood happens on local roadways. The Bike Plan designates the 142 Street east side service road as a district connector. There are currently no other bike routes in Parkview East. The Bike Plan notes that neighbourhood bike routes should be planned at the local level. Therefore, connectivity within the project area will need to be considered to improve biking links through Parkview East.



People of all ages and abilities bike in Edmonton's neighbourhoods

The current Active Transportation Implementation Acceleration (ACTiA) project includes a new shared pathway along 95 Avenue from 142 Street to 163 Street. That link will improve the bike network connectivity from the project area to the west.

Providing new links or upgrading existing links to the river valley trail network will also be considered in this renewal project. There is an opportunity to improve neighbourhood connectivity to the river valley trail system and areas to the east, including Hawrelak Park. Bike routes within the project area will take into account the overall bike network connectivity.

PROJECT CONSIDERATIONS

Expand and improve active transportation connections within the neighbourhood by applying The Bike Plan, Complete Street Design and Construction Standards, and the Safe Mobility Strategy to introduce continuous walking, rolling, and biking routes. Evaluate opportunities to introduce protected or traffic-calmed active transportation corridors that consider winter maintenance to provide year-round mobility for people of all ages and abilities.

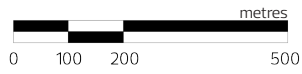
Enhance neighbourhood entry points, increase access to green spaces, and create links to the overall Edmonton bike network and top-of-bank open spaces. Improve active transportation links within the neighbourhood. Evaluate options for improving the 142 Street district connector bike route by considering different facility types and reviewing right-of-way operations at complex service road intersections. Consider wayfinding options, and determine if additional lighting is warranted or would increase comfort and safety of people in the neighbourhood.



Separated pathways in Edmonton can accommodate many types of active modes through all seasons



Map 11 – Active Transportation Network



Map Legend

- Project Area
- Sidewalk
- External Sidewalk Connection
- District Bike Route
- Future District Bike Route
- Granular Pathway
- Other Trail
- Roadway Greens

Traffic Safety

All roads within the project area are classified as local roads. The speed limit for all the project area roads, including the service road, is 40 km/h. Traffic counts completed by the City of Edmonton indicate that all roads in the project area typically have less than 1000 vehicles per day.

140 Street is the only roadway within the project area that has speed survey data. The data was gathered from 2022–2024 and shows an average speed compliance of about 84%, which is generally considered good compliance. Approximately 1% of vehicles on 140 Street exceeded the speed limit by more than 10 km/h.

The City of Edmonton's Safe Mobility team undertook a Street Lab program in 2024 in the project area. After gathering community feedback and completing a safety review of this area, the City installed temporary rubber speed humps on 140 Street (from Buena Vista Road to 89A Avenue), and temporary curb extensions on 89A Avenue at 140 Street (both eastbound and westbound) and on 91 Avenue at 140 Street (eastbound only). At the end of 2025, the City decided to remove the temporary speed humps on 140 Street, which will be removed in 2026. The Street Lab study found that the speed humps did not substantially change driver speeds because speed limit compliance was already good before the speed humps were installed. However, the curb extensions were found to increase pedestrian visibility, comfort, and safety, and will remain in place until Neighbourhood Renewal. It is expected that Neighbourhood Renewal will consider curb extensions at these and other locations.

Safe Mobility has gathered data related to crashes that have occurred within the project area. Although there were several collisions along 142 Street, there were relatively few within the local roads in the project area. The collisions were generally minor in nature with no fatal collisions. Most collisions within the project area included collisions with parked vehicles, and many of them occurred during snowy, icy, or wet conditions.



Temporary speed bumps were part of Edmonton's Street Lab on 140 Street

Including incidents on 142 Street, crashes tend to spike during the morning peak, around noon, and during the evening peak – which saw the most crashes. Throughout the year, crashes occur more frequently during January/February, and July/August with a smaller peak later in the year in November. There is nothing remarkable about these data points as these trends are widespread and correlate with typical daily traffic volume and seasonal changes.

142 Street is an arterial road and the west boundary of the project area, and Buena Vista Road is a collector road and the south boundary of the project area. Since these roads carry significant traffic volumes (particularly 142 Street, which also has a higher speed limit), consideration is required to improve pedestrian and bicycle crossing locations. Three pedestrian-actuated signals are located along 142 Street at Valleyview Drive, 91 Avenue, and 89 Avenue. There is also a Rectangular Rapid Flashing Beacon (RRFB) on Buena Vista Road at 140 Street. The City of Edmonton's Safe Mobility team is planning to install a new RRFB on Buena Vista Road west of the Valleyview Crescent intersection. Safe Mobility also has suggested

considering pedestrian-focused improvements to the 142 Street crossing at 89A Avenue.

For the Neighbourhood Renewal project scope several improvements will be considered to address user safety.

Many of the existing roads do not meet current standards for roadway design. During Neighbourhood Renewal construction there is the opportunity to adjust roadway widths to align with current standards. Adjusting road widths could allow for additional landscaped boulevards between the walkway and roadway, which improves pedestrian safety and comfort, connectivity to transit and within the neighbourhood, and provides space for snow storage.

Following up on the completed Street Lab study, curb extensions will be considered at intersections throughout the project area. Parkview East contains many roads with straight alignments that provide long corridors of uninterrupted traffic flow which are key areas to introduce traffic calming measures to slow traffic and increase pedestrian visibility to vehicles. Locations with high active mode traffic will also be considered for additional improvements.



Temporary curb extensions on 89A Avenue were part of the Street Lab and will be reviewed during Neighbourhood Renewal

Currently the intersections with 142 Street (and its service road) are stop controlled for vehicles traveling eastbound and westbound, and are free-flow on 142 Street. The service roads carry two-way traffic and are also stop-controlled for northbound and southbound traffic on the service roads at the intersection. At two intersections (Valleyview Drive and 91 Avenue), there are pedestrian actuated signals to cross 142 Street. However, pedestrians are still required to cross 142 Street in multiple steps – crossing both the service road and 142 Street. Redesigning the intersections could remove some of the vehicle conflicts, and could simplify the pedestrian crossing, thereby improving safety. A redesign of these intersections, combined with adding boulevard space could further help improve the pedestrian connectivity.

Current City guidelines, including the Complete Streets Design and Construction Standards, indicate that local roads do not typically require separate bike facilities like bike lanes or shared pathways. As Parkview East only contains local roads, there will be consideration to ensure that safe and comfortable routes will be provided for people who bike. This may include combined facilities such as local bikeways, or could be other treatments such as shared pathways or dedicated bike paths or bike lanes. Options will be reviewed and assessed to determine the best solution for the scope of the project.

PROJECT CONSIDERATIONS

A primary consideration for improving traffic safety is to align road widths with the current guidance from the Complete Street Design and Construction Standards. This can also provide space for other uses, such as pedestrian facilities, landscaping, and snow storage. Intersection improvements within the project area and along project boundaries will be considered to improve the safety of all users. Active mode improvements will be considered within the context of the project area and scope.











Road design will need to consider many types of active mode users throughout Parkview East



Map 12 – Intersections + Existing Traffic Calming



Map Legend

-  Project Area
-  Adaptable Curb Extensions (Street Lab)
-  Adaptable Speed Humps (Street Lab)
-  Stop Control (1-way, 2-way, or 3-way)
-  Yield Control (1-way or 2-way)
-  Pedestrian Actuated Signal
-  Pedestrian Actuated RFB
-  Roadway Greens

Infrastructure

Street Lighting

Street lighting is supplied via dedicated poles and underground cables. Alley lighting is attached to power poles. Luminaires have been upgraded to LED throughout much of the neighbourhood, but poles were re-used. Street lights will be replaced with consideration given to lighting levels at intersections, streets, pathways, and within open spaces with consideration of the River Valley Bylaw. Street lights in Parkview East typically do not have pedestals, so there may be some flexibility to adjusting street light locations. Project area residents will also have the opportunity to pursue an option for decorative street light poles.

Water

EPCOR provides water and fire protection services in Parkview East. Water mains are located primarily within alleys. However, for lots without adjacent alleys, the mains are located in the street. Road design will have to consider proximity of catch basins and surface infrastructure to watermains. The project team will coordinate with EPCOR to confirm if a condition assessment is required and any rehabilitation work will be scheduled to occur before or during renewal work.

Storm Drainage

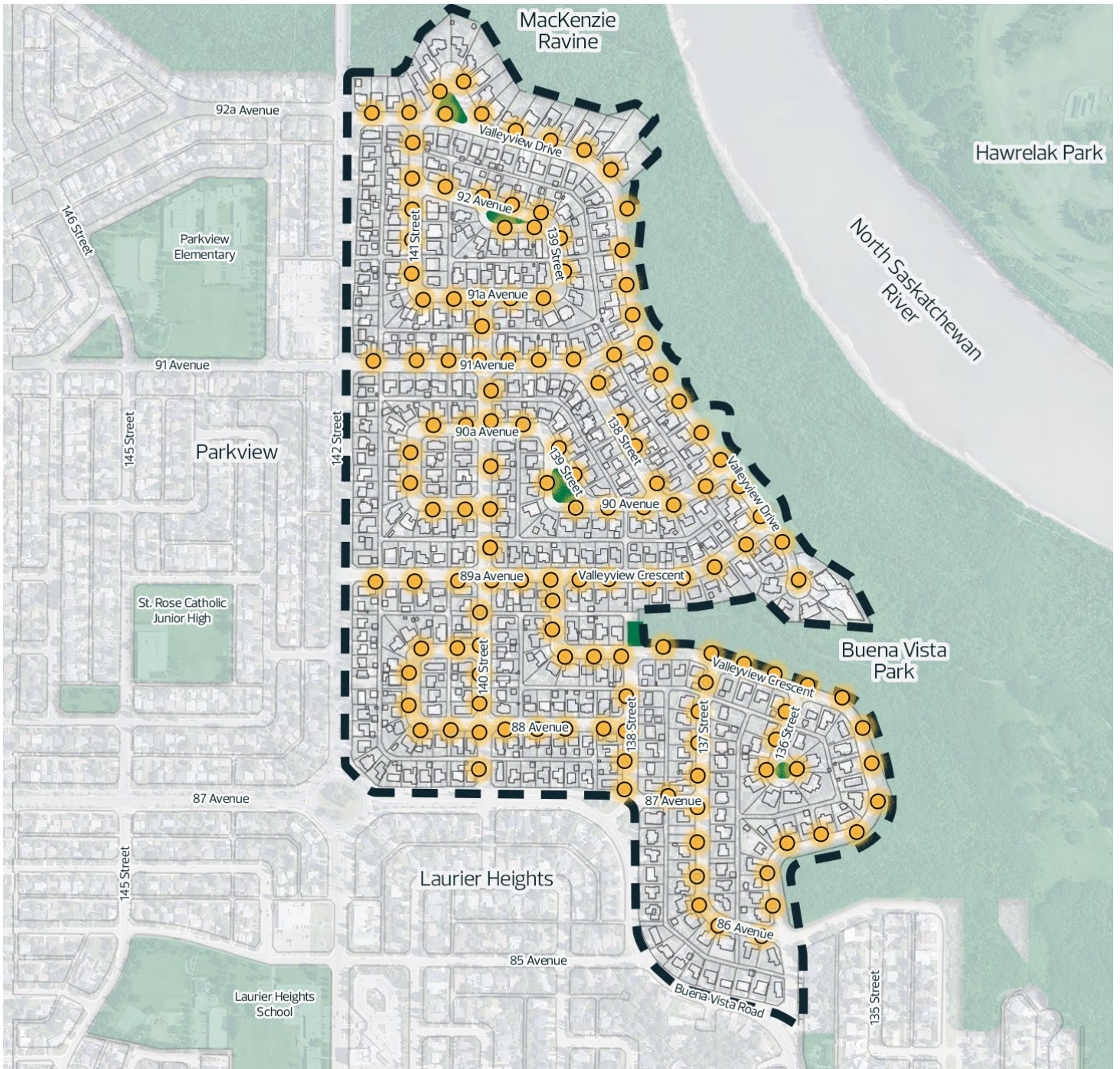
Parkview East has a separate storm sewer system which is owned and managed by EPCOR. Storm pipes and catch basins are primarily located in the streets although there are catch basins and storm pipes in some alleys. The Parkview East drainage system accommodates stormwater from west of 142 Street and carries this, in addition to water collected within the neighbourhood, to nearby outfalls into the North Saskatchewan River. The project team will confirm with EPCOR to determine if a condition assessment or upgrades are required. EPCOR has provided flood risk information which may help plan LID locations within Parkview East. There is also a minimum setback for LID facilities from the top of bank.

Sanitary Sewers

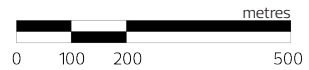
Parkview East has a separate sanitary sewer system which is owned and managed by EPCOR. The sanitary sewer pipes are primarily located in the alleys, but there are some pipes in the roadways. The project team will confirm with EPCOR to determine if a condition assessment or any upgrades are required prior to Neighbourhood Renewal.

Natural Gas + Other Utilities




There are multiple shallow utilities located in the project area to service the neighbourhood. Low pressure gas lines, owned by ATCO, are generally located in alleys, but there are some pipes within the roadways. Telecommunication lines, owned by multiple providers, are typically located overhead on power poles in alleys but some are buried in the front of lots. Similarly, the majority of power is supplied via overhead distribution lines in alleys, with limited areas serviced by underground cables in the street. Existing utilities will be located and any potential conflicts will be identified. The project team will coordinate with individual utility owners to determine any necessary changes or upgrades in the project area. Utility adjustments will be scheduled to occur before or during the renewal construction.



Map 13 – Street Lighting Infrastructure



Map Legend







-  Project Area
-  Street Light
-  Roadway Greens



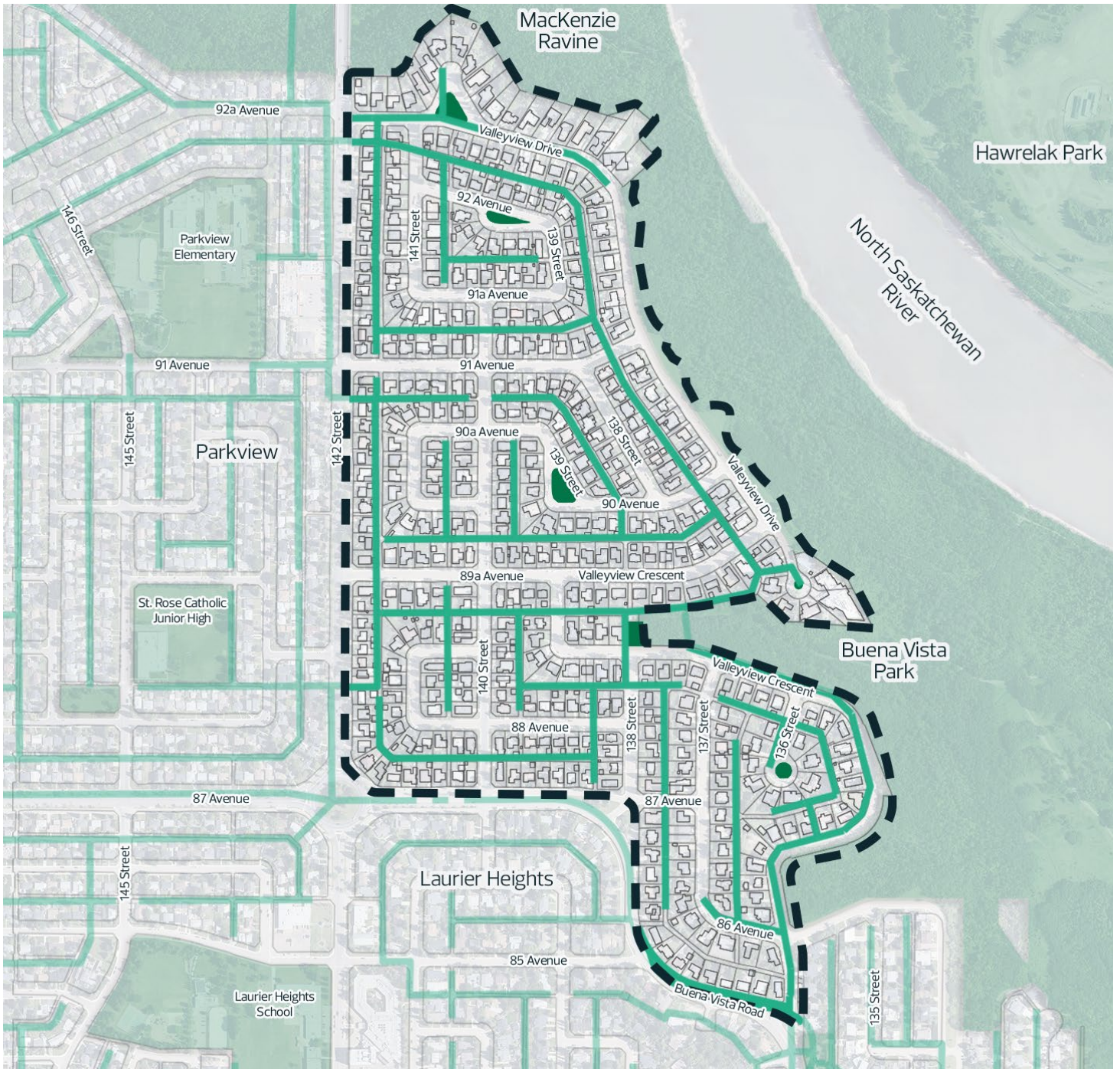
Map 14 – Stormwater Infrastructure



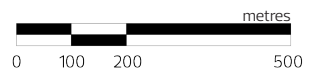
Map Legend

-  Project Area
-  Stormwater Main
-  Potential Surface Ponding
High Risk of Occurrence*
-  Potential Surface Ponding
Moderate Risk of Occurrence*
-  Roadway Greens
-  Top of Bank Setback
(LID Restrictions)




*Surface Ponding Risk data provided by EPCOR

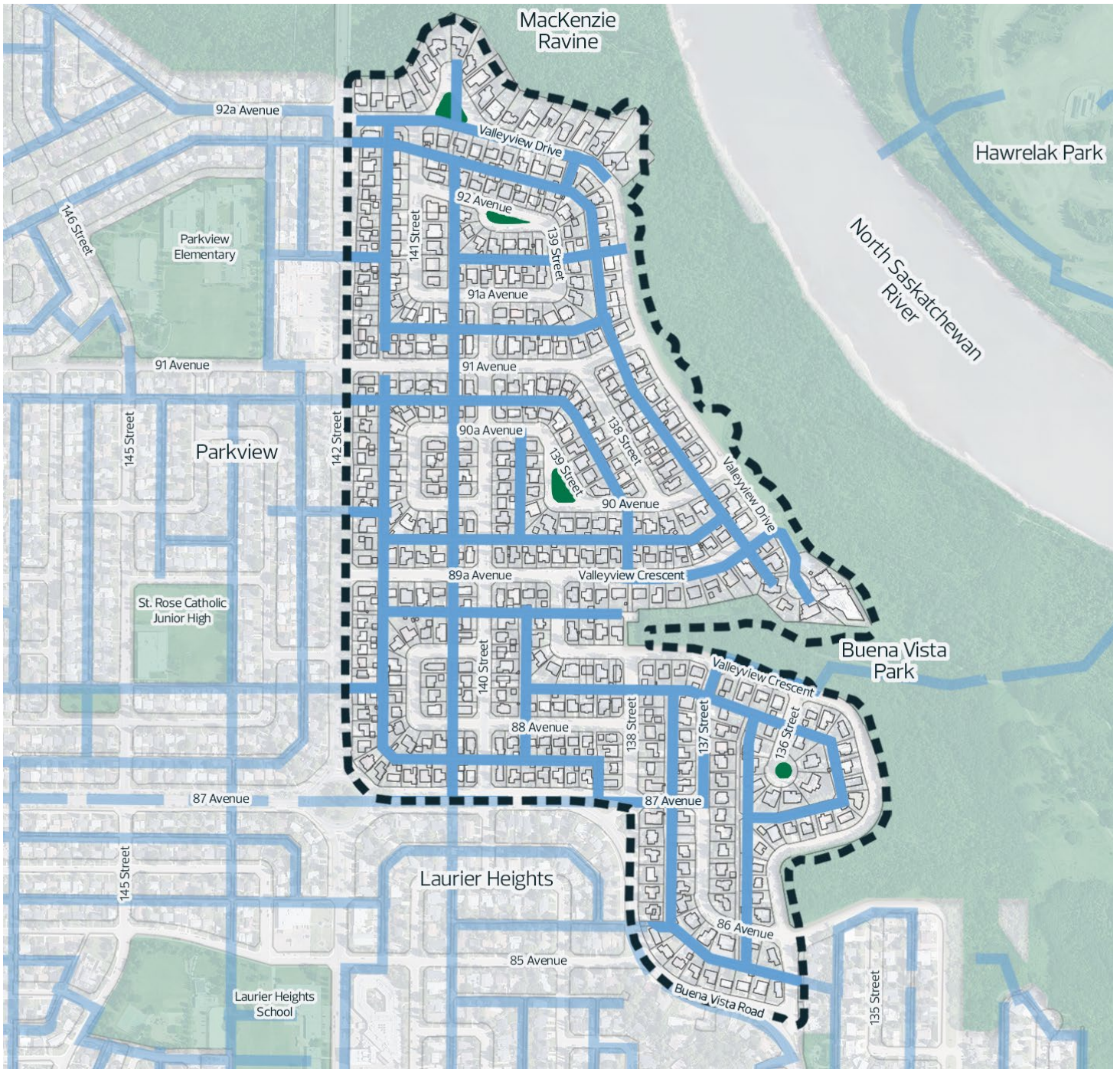


Map 15 – Sanitary Sewer Infrastructure

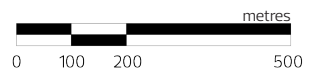


Map Legend




-  Project Area
-  Sanitary Main
-  Roadway Greens

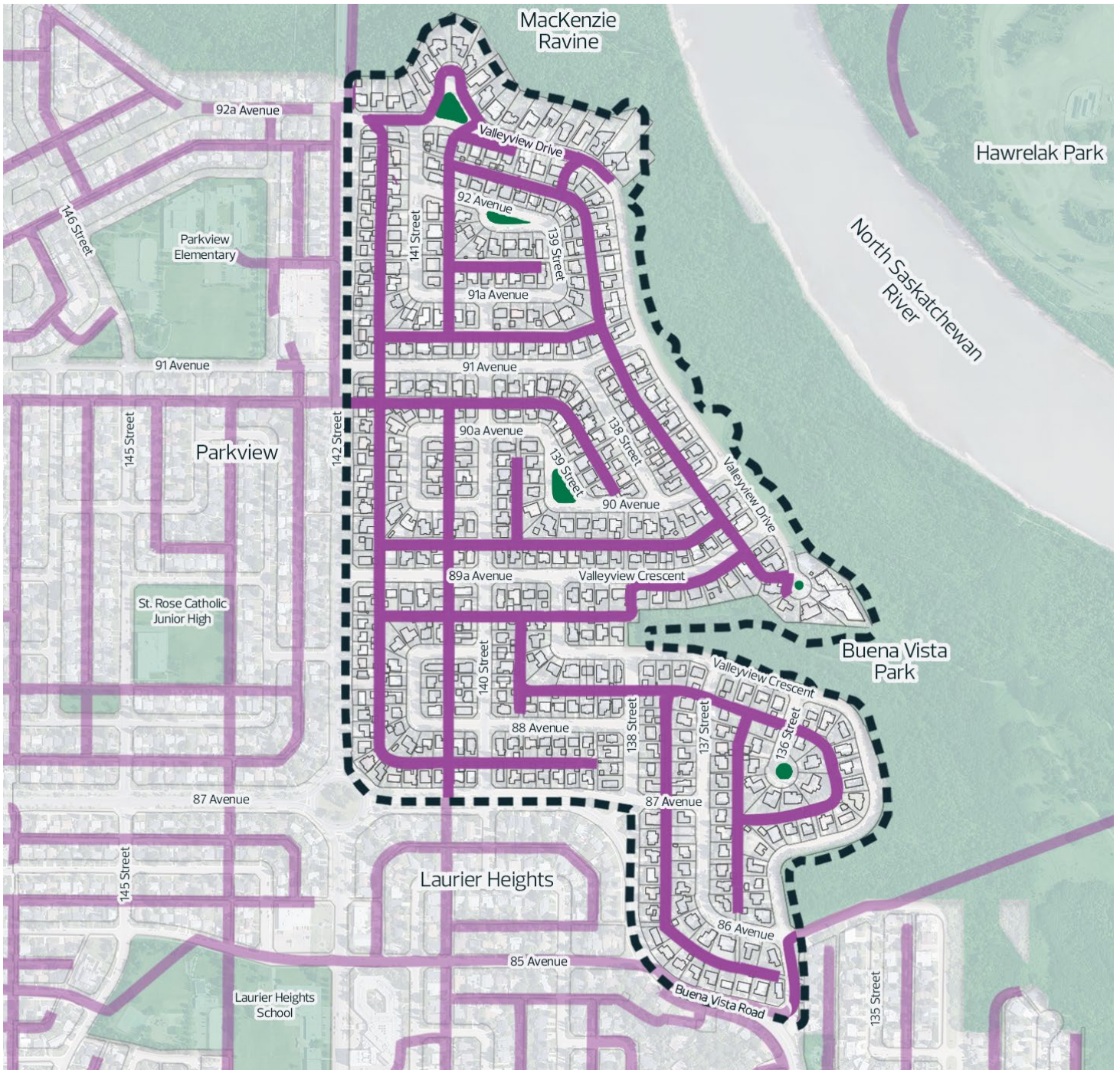


Map 16 – Water Infrastructure






Map Legend

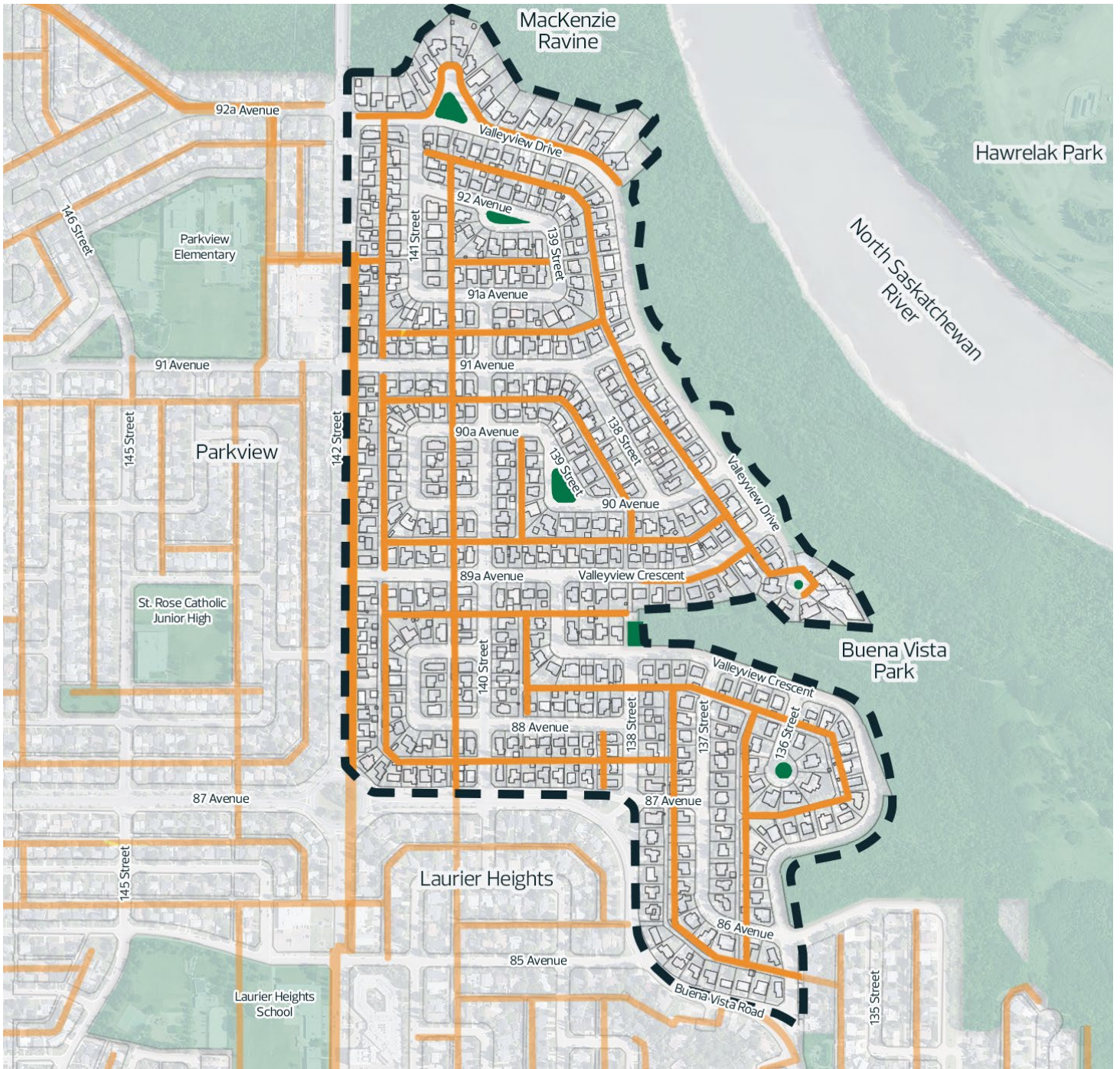
-  Project Area
-  Water Main
-  Roadway Greens



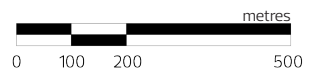
Map 17 – Power Infrastructure

Map Legend




-  Project Area
-  Power Lines
-  Roadway Greens



Map 18 – Natural Gas Infrastructure



Map Legend

-  Project Area
-  Natural Gas Lines
-  Roadway Greens