



# Toward a New Vision for Jasper Avenue

February, 2011

**URBAN  
STRATEGIES  
INC.**

**PHILLIPS · FAREVAAG · SMALLENBERG**  
PLANNING · URBAN DESIGN · LANDSCAPE ARCHITECTURE

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and Land Services





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# Acknowledgements

*“We thank all of those who contributed to the study process.”*

The Jasper Avenue New Vision Final Report is the result of the efforts of many individuals including the City of Edmonton, downtown business owners, residents, property managers, developers, designers, and city councillors. The ideas and thoughts expressed by those who have taken an active interest in the study have resulted in an inspiring and compelling new vision for Jasper Avenue’s streetscape, as well as a range of important city-building initiatives that will surround it.

We would like to thank all of those who have shared their input through meetings and stakeholder working sessions, contributed and participated in the Ideas Workshop and Community Open Houses, and/or have simply taken the time to share their ideas and aspirations for the future of Jasper Avenue and their downtown with the project team.

# Consultant Team



- Project lead and management
- Urban design and public realm
- Consultation and facilitation
- Planning analysis and recommendations



- Landscape architecture
- Conceptual streetscape design



- Transportation and engineering
- Engineering concept and preliminary design

## **Armin A. Preiksaitis & Associates Ltd.**

- Local planning policy
- Implementation recommendations

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▲ Figure 1  
Illustrative rendering of the revitalization of Jasper Avenue

# Executive Summary

The New Vision for Jasper Avenue sets out a plan to revitalize Jasper Avenue as the City of Edmonton's premier main street. The document is presented in five parts:

**Part One: Background to the Study** outlines the purpose of the study and broad municipal goals, introduces the project team, describes the study area, and presents an overview of the study process and events associated with the work program and the consultation process. The study was led by Urban Strategies Inc. with Phillips Farevaag Smallemberg, ISL Engineering and Armin A. Preiksaitis and Associates. The study featured 5 major consultation events.

**Part Two: Setting the Stage for Renewal and Revitalization on Jasper Avenue** provides an overview of the evolution of Jasper Avenue and its characteristics at the level of the street, its context within the downtown, and role within the city. This section also outlines an Opportunity Framework, highlighting key city-building opportunities within the study area and finally, summarizes the results from the Ideas Workshop, a key consultation event which laid the foundation for the New Vision Plan.

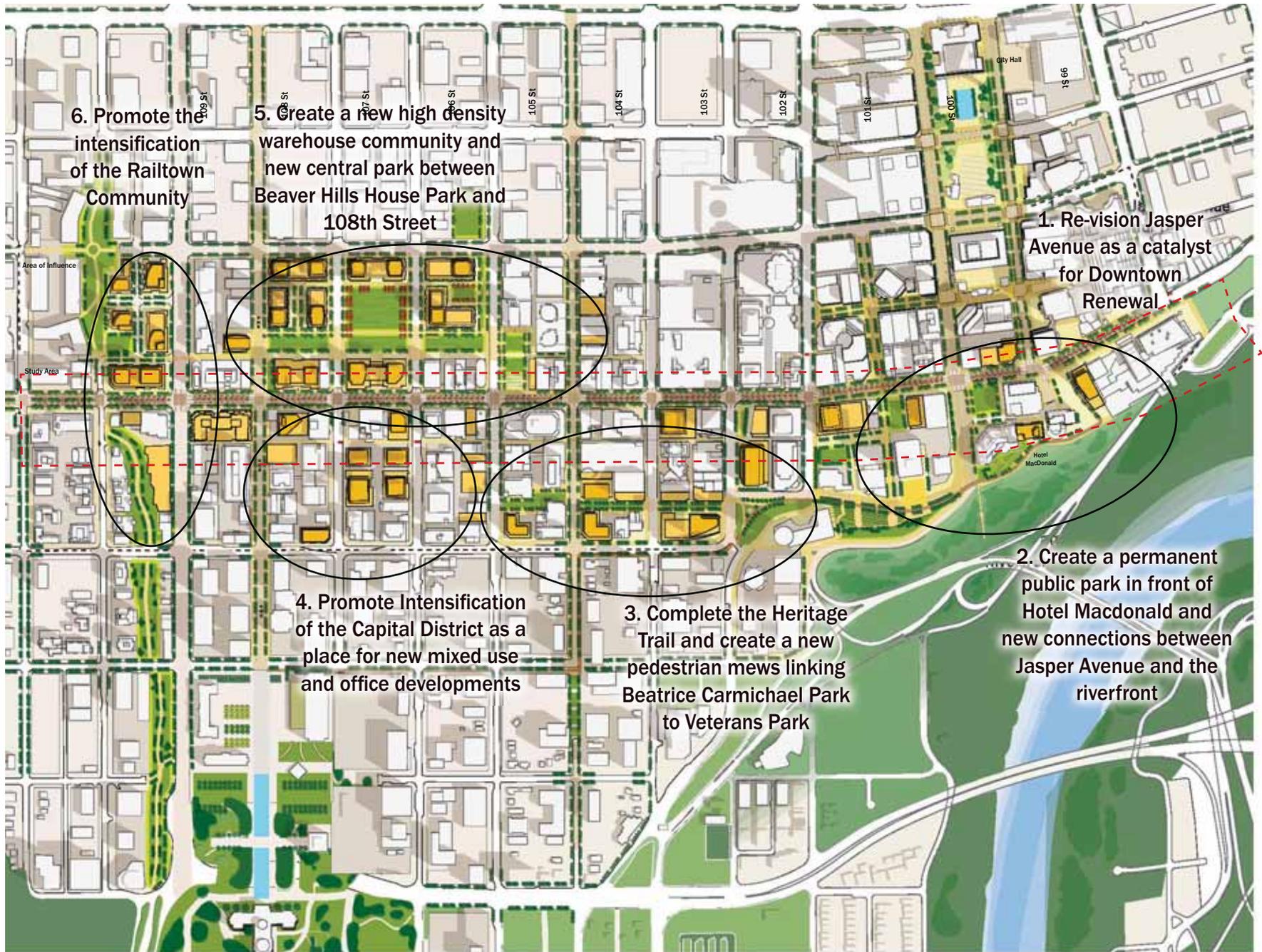
This investigative phase revealed some key objectives to be addressed in the New Vision, including the need to:

- reconfigure Jasper Avenue's Right-of-Way (ROW) to emphasize pedestrian movements, narrowing the roadway and widening the sidewalk on this important transit and pedestrian thoroughfare;
- address engineering challenges associated with a reconfiguration of the ROW;
- update and refresh the deteriorated and dated streetscape of Jasper Avenue;
- develop urban design guidelines to best shape future infill and development projects along along the street;
- create a more continuous, active retail environment;
- increase downtown's residential population to support the revitalization of Jasper Avenue and retail development expansion; and,
- improve public realm amenities.

## **Part Three: Jasper Avenue New Vision: A Framework for Renewal, Reinvestment and Focused City Building**

articulates a broadly accepted vision for future of Jasper Avenue, the streetscape and the surrounding physical context. It builds carefully on existing assets and opportunities within the study area and creates a range of incremental key moves to encourage and guide desired reinvestment and development in downtown Edmonton over a 25 year time period. The New Vision makes recommendations for a range of additional public realm improvement strategies which should be prioritized and undertaken over time and provide a new high quality setting for a range of mixed use and residential development opportunities. When realized, the New Vision has the potential to bring:

- Approximately 15,000 new residents to the downtown;
- Over 92,900 square metres of new office space;
- 60,387 square metres of new street related retail space;
- 3.7 hectares of new park space;
- \$19 million (2010 dollars) in new tax revenue;
- New mixed use and high density residential communities on and adjacent to Jasper Avenue; and,
- High quality public realm and built form demonstrating excellence in downtown city building.



▲ Figure 2  
Six Big Moves for the Revitalization of Jasper Avenue

### Vision Principles:

The following Vision principles outline broad objectives for the renewal of the study area, which should be used to guide development, investment and long term city-building:

- Leverage the Jasper Avenue streetscape initiative as a catalyst for downtown rejuvenation;
- Create an attractive and distinctive streetscape for Jasper Avenue, which prioritizes pedestrian, transit and civic uses throughout the year;
- Encourage more people to live on Jasper Avenue in order to support its main street commercial functions and downtown vitality beyond the work day;
- Focus on place making – not projects;
- Create a connected network of new and improved park spaces and streetscapes;
- Expand and diversify the commercial draw of Jasper Avenue;
- Protect and strengthen assets along Jasper Avenue; and,
- Strive towards high quality design excellence.

### The Six Big Moves

The following Six Big Moves describe the major thrusts of the New Vision and the extent of change for major development areas in the study area:

1. Re-vision Jasper Avenue as a catalyst for continued downtown development and re-investment;
2. Create a permanent public park in front of Hotel Macdonald and reinforce connections between Jasper Avenue, the riverfront and the civic district through a range of Civic Precinct public realm recommendations;
3. Complete the Heritage Trail along the river valley edge and create a pedestrian mews linking Beatrice Carmichael Park to Veterans Park;
4. Promote intensification of the Capital District through new mixed use and office developments;
5. Use the largest underutilized and vacant redevelopment sites to make a big impact downtown: create a new high density warehouse community and new central park, just north of Jasper Avenue between Beaver Hills House Park and 108<sup>th</sup> Street; and,
6. Promote the continued intensification of the Railtown Community as a transit-supportive, mixed use neighborhood.



▲ Figure 3  
Illustrative rendering of the revitalization of Jasper Avenue



▲ Figure 4  
Foam core model of the revitalization of Jasper Avenue

#### **Part Four: Policy, Urban Design and Other Strategies**

This section of the document outlines a range of policy, urban design and other recommendations in support of the New Vision. These recommendations are intended to support the intent of Capital City Downtown Plan (CCDPlan) approved by Council on July 7, 2010, as well as provide additional policy and urban design recommendations aimed at achieving a high quality of city building within the study area. Both the municipality and development community are encouraged to utilize these recommendations to guide development projects and decision making in the area. However, it should be noted that the policies outlined in the CCDPlan shall take precedence over the Jasper Avenue New Vision recommendations. Municipal staff will review the policy and urban design recommendations outlined in this study, which may be partially or wholly integrated into a future update of the Downtown Capital District Plan and or Downtown Zoning By-law.

**Land Use Recommendations** outlines the range of land use strategies for the area, including expansion of retail and commercial uses along Jasper Avenue and elsewhere within the study area. It is important to note that the CCDPlan was approved by Council on July 7, 2010. The policies and zoning in the CCDPlan shall take precedence over the Jasper Ave. New Vision' Land use recommendations.

**Key recommendations include:**

- Defining a retail hierarchy to help vitalize Jasper Avenue and the downtown with supporting policy recommendations.

**Urban Design Guidelines** provides criteria and recommendations to guide new development in the study area and the retrofit of existing structures to ensure a vibrant and visually appealing built environment. Urban design guidelines will result in an improved quality of place throughout the study area and will foster the evolution of Jasper Avenue as a high value shopping district.

**Key recommendations include:**

- Ensuring that each new building or the retrofit of buildings contribute positively to the goal of

re-establishing Jasper Avenue as a signature commercial main street;

- Built form recommendations to achieve a high quality of distinctive place-making in the downtown;
- Built form guidelines for tall buildings, as well as mid-rise developments;
- An emphasis on podium and tower development forms to reinforce the pedestrian street wall, while providing density in slim tower forms, which allow light and sun penetration

**Mobility Recommendations** outlines policy recommendations to effectively accommodate all modes of movement within the study area.

**Key recommendations include:**

- Prioritizing active modes of transportation, including walking and cycling;
- Effectively addressing service functions and the management of parking; and
- Addressing universal access.

**Part Five: Implementation** outlines an implementation framework including recommended policy changes. It also provides recommendations for financing the Jasper Avenue streetscape improvements, acquiring park spaces, managing parking, strengthening commercial diversity and other elements necessary to support the New Vision and initiate positive change along Jasper Avenue.

**Key recommendations include:**

- Embedding New Vision policy recommendations in the CCDPlan and Zoning By-law;
- Implementing the Streetscape Improvement Plan through 100% City funding; and
- Increasing parks spaces within the study area and downtown to create amenities to encourage private sector reinvestment and a more livable, attractive and connected downtown environment.



▲ Figure 5  
Illustrative rendering of the revitalization of Jasper Avenue

TOWARD A NEW VISION FOR JASPER AVENUE

# Introduction: A Vision for the Future

*“It is time to establish a New Vision for Jasper Avenue that can renew its role as Edmonton’s premier civic main street; a focus for economic, cultural, community and civic life in the heart of the city.”*

This document establishes a new streetscape, public realm and urban design vision for Jasper Avenue – a vision that can successfully guide the evolution and transformation of Jasper Avenue and restore its role as Edmonton’s signature downtown main street. Historically, Jasper Avenue fulfilled this role, but in recent decades it has diminished due to the aging and deteriorating condition of the streetscape, rapid suburban expansion and significant competition from major shopping outlets beyond the downtown.

Throughout the course of this study, it has become evident that many Edmontonians have a strong desire to see Jasper Avenue transform as a dynamic place for the celebration of Edmonton’s social, economic and cultural vitality; a place of great pride showcasing the accomplishments, values and beauty of the city and a place for main street and leisure activities supporting healthy downtown neighbourhoods. Jasper Avenue needs to evolve as a well designed, beautiful, memorable and pedestrian-friendly destination that both residents and visitors are drawn to time and time again.

This study provides the basis for guiding renewal, reinvestment and community building initiatives along Jasper Avenue over the next 25 years. It is a strategic document that will be used by the City of Edmonton, the downtown business, landowners, developers and residential communities as a tool to go help develop, guide and implement future public and private sector investment projects in the downtown area. The goal is to nurture a more vibrant, successful and desirable place through a series of incremental city-building actions that will improve the physical setting, amenity and quality of Jasper Avenue.

The underlying principle of this study is that the revitalization of Jasper Avenue streetscape can serve as a catalyst for private sector reinvestment, continued renewal and positive change in the corridor. To fully realize this goal, the streetscape improvements must be accompanied by a range of other targeted investments in the study area undertaken by both the public and private sector. These improvements are intended to strengthen the commercial draw of the area, attract a significant amount of new residents and shoppers, improve the character and sense of place along this main street and create new open spaces.

There will always be more cultural diversity, history, character, architectural interest, tourism potential, and economic opportunity in the core of the city than anywhere else. Edmonton’s downtown holds tremendous promise for a bright, thriving and prosperous future and the Jasper Avenue New Vision initiative creates a clear strategy to realize this potential.

## The New Vision

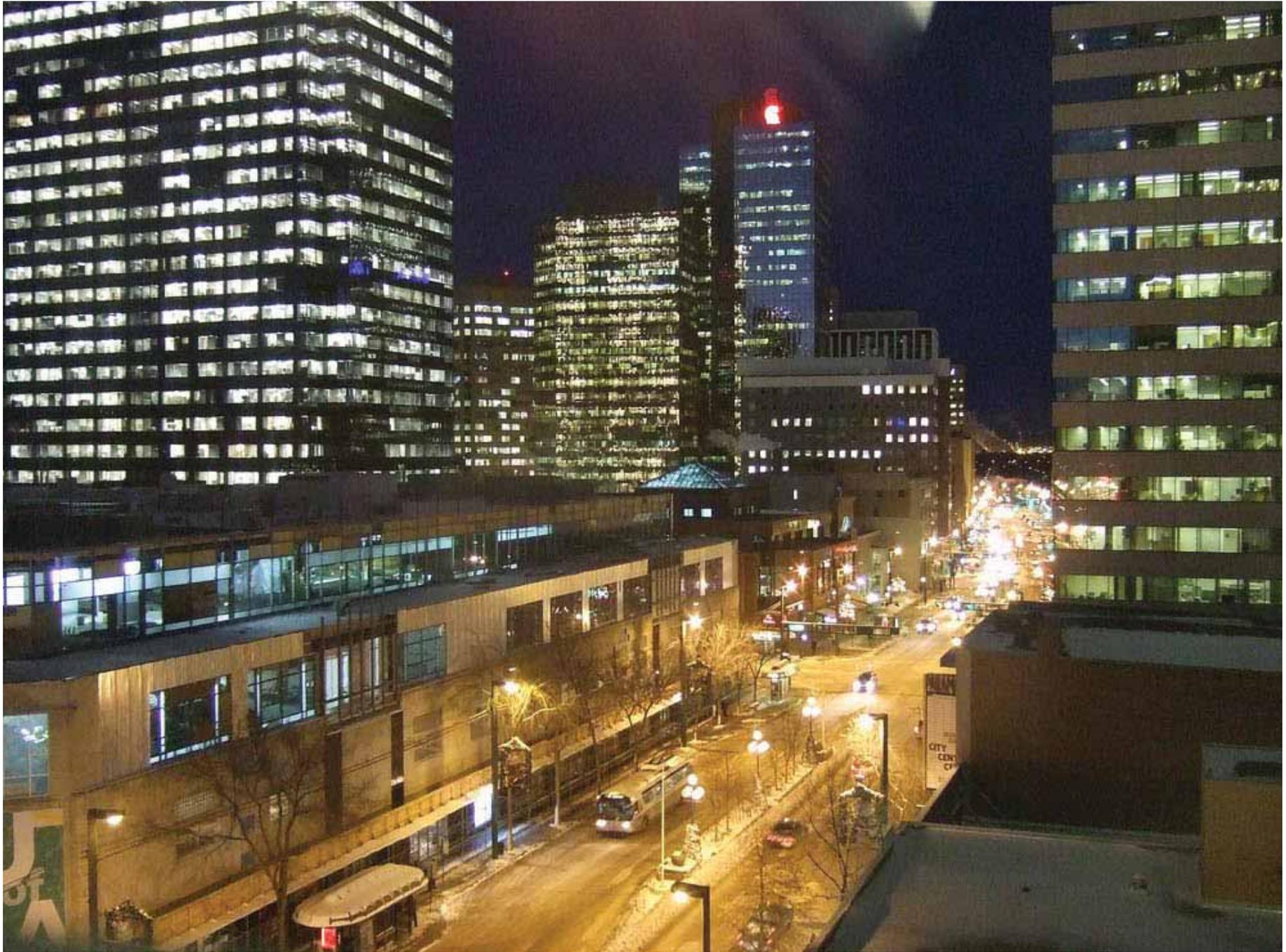
The New Vision for Jasper Avenue will:

1. Create a shared vision for the renewal of Jasper Avenue - a vision that appropriately reflects the aspirations of the downtown community, City of Edmonton, business and institutional leaders, developers, and members of the public who participated in the study;
2. Build logically on the area's existing strengths, assets and opportunities;
3. Respond appropriately to contemporary urban realities and market conditions;
4. Describe the plan for a renewed Jasper Avenue streetscape, which will be utilized to inform the preparation of detailed construction documents;
5. Establish policy and urban design recommendations, which support the implementation of the vision and will assist the municipality in achieving a high quality downtown environment. Policy and urban design recommendations should be reviewed and integrated within the CCDPlan and Zoning By-law to achieve the goals of the vision;
6. Describe opportunities for a range of public realm improvements, which should be prioritized and implemented over the the next 25 years;
7. Provide certainty about the future and the roles that both the municipality and the private sector will play in implementing priority initiatives needed to kick start longer term public and private sector investment; and,
8. Demonstrate the financial return through taxation benefits of a focused city-building program in the downtown core.



*The New Vision for Jasper Avenue is more than a streetscape initiative. It is about identifying a range of targeted and mutually supportive initiatives aimed at transforming the downtown over the next 25 years.*

◀ Figure 6:  
Potential view of Jasper Avenue, looking east



▲ Figure 7  
Existing Condition of Jasper Avenue

# Part One: Background to the Study

## 1.1 Study Purpose and Key Municipal Goals

The current Jasper Avenue streetscape design was developed in the mid 1980s in an attempt to better support commercial viability along the street in light of commercial growth at the edges of the City. Contemporary in its day, the streetscape is now approximately 25 years old and appears overly cluttered, dated and deteriorated. In 2008, the City of Edmonton determined that repair work was necessary for the roof of the Central LRT station, which would necessitate the removal of substantive streetscape and surface elements in the vicinity of the station area. The municipality viewed this as an opportunity to address current streetscape deficiencies along Jasper Avenue and put in place a new streetscape plan, of which, the Central LRT station segment, from 100th Street to 102<sup>nd</sup> Street, could occur in conjunction with the scheduled 2012 roof repair. The CCDPlan has recognized the need to revitalize Jasper Avenue, as the downtown's primary shopping main street. Specifically, it has identified this project as an important catalyst to:

- Increase the number of people living downtown;
- Enhance the unique neighbourhoods located in the downtown; and
- Link the downtown neighbourhoods together, as well as to the rest of the city.

Collectively, these factors have led to the Jasper Avenue New Vision initiative. The New Vision initiative will establish a coherent urban design framework and new streetscape design to guide the revitalization and evolution of the corridor as a more pedestrian-oriented, attractive and vibrant signature commercial main street.

### **Key objectives for the study are to:**

1. Identify broad community, business and development goals and expectations;
2. Develop a comprehensive Vision for the Jasper Avenue corridor which identifies future built form and open space development opportunities aimed at improving the quality and amenity of the area;
3. Make recommendations to strengthen the commercial character of Jasper Avenue and the surrounding area. Specifically, aim to recapture Jasper Avenue's role as downtown Edmonton's premier main street and shopping destination;

4. Recommend policies and guidelines to achieve the goals of the New Vision;
5. Establish a streetscape concept for Jasper Avenue between 97<sup>th</sup> Street and 111<sup>th</sup> Street;
6. Translate the Jasper Avenue streetscape concept into a functional plan for the blocks between 97<sup>th</sup> Street and 103<sup>rd</sup> Street, in order to be used to deliver the phase 1 streetscape concept, as identified in Section 3.4 of this report, in conjunction with the Central LRT repair; and
7. Recommend a high level implementation strategy to achieve the goals of the plan, and to inform future municipal actions and projects.

Underpinning each of these key objectives is a commitment on the part of the City of Edmonton to achieve a realistic plan that can bring about coordinated public sector actions and improvements over the short and medium term.



▲ Figure 8  
Organization of the Study Team

## 1.2 The Study Team

In November 2008, a multi-disciplinary consulting team was selected to work with the City of Edmonton, residents, land owners, business owners, development interests and other stakeholders to undertake the New Vision study for Jasper Avenue. The study team was led by Urban Strategies Inc., a leading Canadian firm in the areas of planning, urban design, consultation and facilitation. Urban Strategies were responsible for the creation of the overall vision for the Jasper Avenue area. The study team also included Phillips Farevaag Smalenberg Landscape Architects, of the City of Vancouver, who were responsible for undertaking the Jasper Avenue streetscape improvement concept. ISL Engineering, from the City of Edmonton, were responsible for the transportation and engineering components of the study. In addition, the Edmonton-based planning firm, Armin A. Preiksaitis and Associates, provided local planning policy guidance, as well as recommendations for implementation.

To ensure that the vision and resulting urban design, policy and streetscape recommendations of the study were realistic and viable, the study team worked collaboratively with a range of City of Edmonton municipal departments, including Planning and Development, Urban Design, Parks and Transportation Services

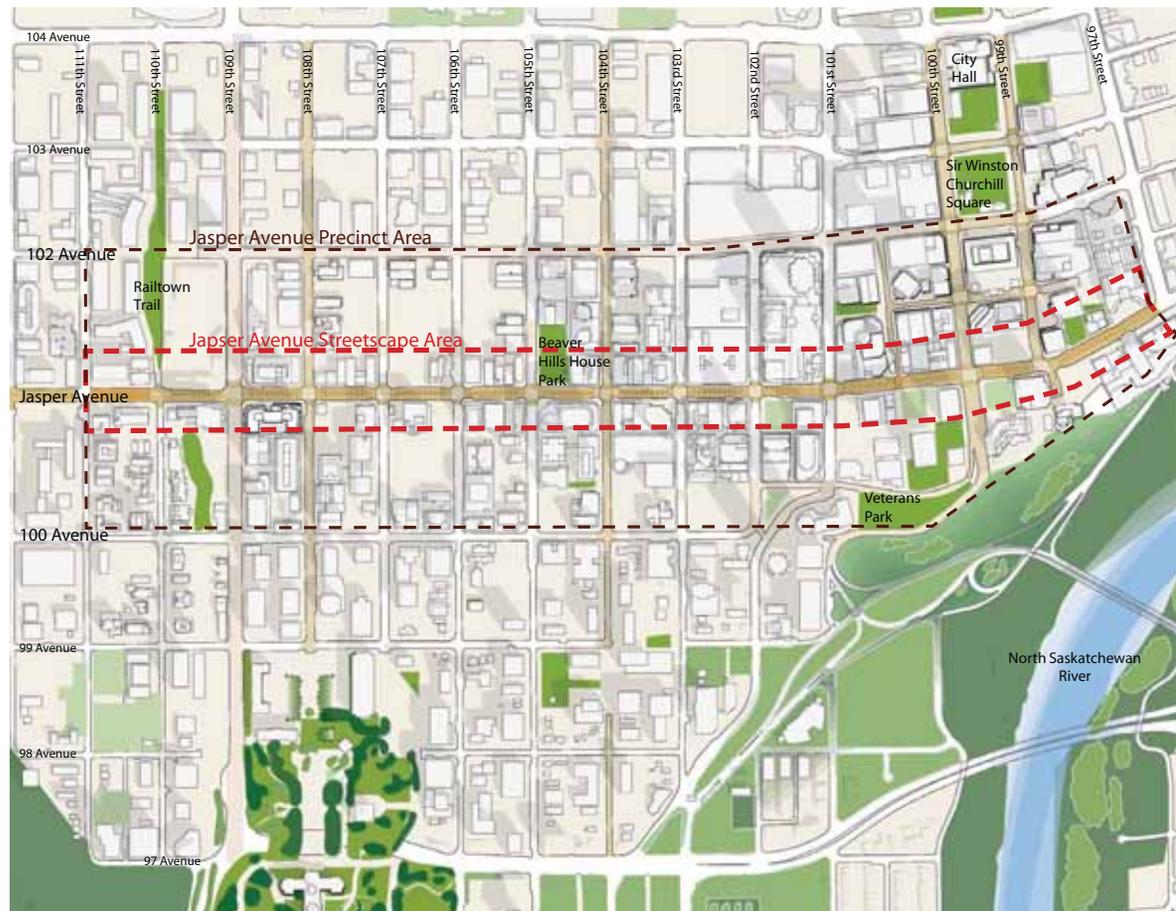
## 1.3 The Study Area

The New Vision study area consists of two geographic areas between 97<sup>th</sup> and 111<sup>th</sup> Streets: the **Jasper Avenue Streetscape Area** and the **Jasper Avenue Precinct Area**.

The **Jasper Avenue Streetscape Area** includes the Jasper Avenue ROW and properties on both sides of the street that front the street up to the depth of the rear laneway, or laneway line in cases where development exceeds the laneway. The study recommendations within the Jasper Avenue Streetscape Area focuses primarily on the Streetscape Concept, which includes

the side streets to the intersection of the rear lanes. Side streets provide important locations for a range of streetscape furnishings, servicing and utility needs, and in some cases, LRT station entrances, which are part of an integrated and comprehensive streetscape concept for Jasper Avenue. In addition, recommendations within this Streetscape Area include key transportation and engineering directions, as well as detailed recommendations for built form improvements to properties fronting Jasper Avenue.

The **Jasper Avenue Precinct Area** includes the full block depth both north and south of Jasper Avenue from 100<sup>th</sup> to 102<sup>nd</sup> Avenues between 97<sup>th</sup> and 111<sup>th</sup> Streets. Jasper Avenue cannot be understood in isolation from its context. The overall health of the Avenue is dependent on the vitality, quality and level of activity both on the street and the immediate context. The relationships between street activity and commercial viability on Jasper Avenue and the vitality of the surrounding neighbourhoods are synergistic and interwoven. A new streetscape on Jasper Avenue can raise the profile of the area and increase interest in downtown development. This, together with other initiatives in the area can create demand for increased



▲ Figure 9  
The Study Area

## 1.4 The Study Process

The Jasper Avenue New Vision Initiative was completed in 5 stages:

**Stage 1:** Project initiation and review of municipal goals; key stakeholder interviews; reconnaissance and background review.

**Stage 2:** Investigation and analysis; Ideas Workshop.

**Stage 3:** Preparation and refinement of the Jasper Avenue New Vision: urban design, streetscape, transportation and engineering components

**Stage 4:** Preparation of the Functional Streetscape Design for Jasper Avenue and detailed policy and built form recommendations

**Stage 5:** Preparation of the final report-  
**TOWARD A NEW VISION FOR JASPER AVENUE**

**Future stages of work:** The creation of detailed design and tender documents for the Jasper Avenue Streetscape Area will be undertaken and phased according to the municipal capital budget.

downtown living and working, which, in turn can create demand for higher quality retail and shopping venues. To realize these benefits, a vision is needed that creates certainty about how the area should change over the next 25 years and how this vision is supported by a policy and action framework. These tools will ensure that each new project or addition to Jasper Avenue and its context are aligned to the New Vision and can positively contribute toward long term and intentional city-building goals.

Recommendations for the Jasper Avenue Precinct Area focus on articulating a bold, comprehensive vision for this part of the downtown and identifying projects and initiatives in several local 'precinct areas', which can revitalize the area. The recommendations are supported by land use, built form and public realm recommendations which may be integrated within

the CCDPlan and Zoning By-law to create a strong framework to guide change. This document articulates the leadership role of the municipality in delivering the New Vision and outlines tools to assist in the implementation process.

Over time, it is possible that the New Vision concept could be extended to the east into The Quarters and west into Oliver to create a cohesive street experience across the length of Jasper Avenue. In addition, many of the land use and built form recommendations of this study may be applied more broadly to other areas of the downtown. The municipality should explore these opportunities as it prepares the planned downtown Zoning By-law update.

## 1.5 The Consultation Process

Public and stakeholder consultation was a key component of the study process throughout the creation of the New Vision. A variety of consultation events provided the project team with a range of perspectives and viewpoints on important issues facing Jasper Avenue and Edmonton's downtown. The consultation events also assisted the project team decode aspirations and hopes for what Jasper Avenue could become.

The consultation process confirmed that this initiative is important for the downtown. Consultation events included the general public, major educational institutions, land owners, developers, business owners, provincial representatives and various municipal departments. In order to make the New Vision a reality, leadership, hard work and dedication is required of many people.



▲ Participants of the Ideas Workshop in April-May, 2009 work with the foam model.

**Land Use Recommendations** outlines the range of land use strategies for the area, including expansion of retail and commercial uses along Jasper Avenue and elsewhere within the study area. It is important to note that the CCDPlan was approved by Council on July 7, 2010. The policies and zoning in the CCDPlan shall take precedence over the Jasper Ave. New Vision' Land use recommendations.

**Key recommendations include:**

- Defining a retail hierarchy to help vitalize Jasper Avenue and the downtown with supporting policy recommendations.

**Urban Design Guidelines** provides criteria and recommendations to guide new development in the study area and the retrofit of existing structures to ensure a vibrant and visually appealing built environment. Urban design guidelines will result in an improved quality of place throughout the study area and will foster the evolution of Jasper Avenue as a high value shopping district.

**Key recommendations include:**

- Ensuring that each new building or the retrofit of buildings contribute positively to the goal of

re-establishing Jasper Avenue as a signature commercial main street;

- Built form recommendations to achieve a high quality of distinctive place-making in the downtown;
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**Mobility Recommendations** outlines policy recommendations to effectively accommodate all modes of movement within the study area.

**Key recommendations include:**

- Prioritizing active modes of transportation, including walking and cycling;
- Effectively addressing service functions and the management of parking; and
- Addressing universal access.

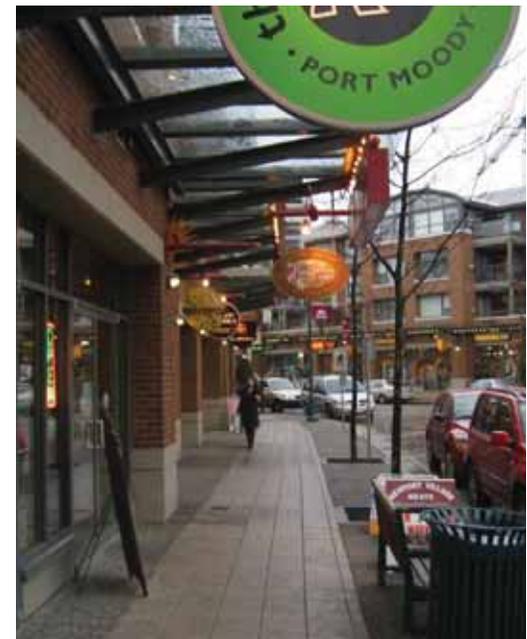
**Part Five: Implementation** outlines an implementation framework including recommended policy changes. It also provides recommendations for financing the Jasper Avenue streetscape improvements, acquiring park spaces, managing parking, strengthening commercial diversity and other elements necessary to support the New Vision and initiate positive change along Jasper Avenue.

**Key recommendations include:**

- Embedding New Vision policy recommendations in the CCDPlan and Zoning By-law;
- Implementing the Streetscape Improvement Plan through 100% City funding; and
- Increasing parks spaces within the study area and downtown to create amenities to encourage private sector reinvestment and a more livable, attractive and connected downtown environment.



▲ Workshop participants expressed a range of goals and ambitions for a revitalized Jasper Avenue.



▲ Pedestrian friendly streetscape conditions

## Part Two: Setting the Stage for Renewal and Revitalization on Jasper Avenue

*All places change. They grow, evolve, stagnate, decline, and rejuvenate. Jasper Avenue needs purposeful change to ensure that it contributes life and vitality to the downtown for generations to come.*

The Jasper Avenue New Vision initiative is occurring at a time when municipalities across North America are looking critically at the performance of their downtowns. The City of Edmonton has recognized the need for a new streetscape plan for Jasper Avenue, in order to coincide with the repair of the Central LRT Station planned for 2012. The New Vision initiative is a significant opportunity to redefine the physical character of Jasper Avenue, as well as look critically at Jasper Avenue's performance, its assets and failings. This initiative will also help develop a streetscape and urban design plan that can strengthen Jasper Avenue's relevance within the downtown and City.

In support of the above, this section will:

- Review the role and evolution of Jasper Avenue over time; and
- Summarize key issues, challenges and opportunities facing Jasper Avenue and surrounding context.



▲ Streetscape improvements in New York City

## 2.1 Historic Overview of Jasper Avenue

Jasper Avenue has a unique and interesting history, tied to the evolution and growth of western Canada. As the City of Edmonton has experienced steady growth up to World War II (WWII), followed by a post-war boom, and finally a period of urban decline. As the City experienced change, Jasper Avenue has remained central to the story.

Jasper Avenue first emerged as a commercial main street in the 1870s and 1880s. In the early 1900s, the railroads and the High Level Bridge further connected the City of Edmonton to the outside world, and the incorporation of Edmonton in 1904 solidified Jasper Avenue's role as the downtown main street. The 1910s saw the amalgamation of the Cities of Edmonton and Strathcona, leaving Edmonton with two main streets

– Jasper Avenue to the north, and Whyte Avenue to the south. Jasper Avenue continued to gain strength as a commercial centre through the 1930s and 40s, supporting streetcars as well as motorized vehicles.

The years following WWII saw unprecedented growth, with the discovery of oil in the City of Leduc in 1948 and the strategic positioning of Edmonton as a base for the major construction projects linked to the oil discovery. Growth continued into the 1960s as the City annexed several of its neighbours, and intensification of the downtown core began. The 1970s and the accompanying oil boom, as well as growth of the Provincial Government, changed the downtown dramatically. In the 1970s, a great deal of historic fabric was demolished in order to accommodate large towers and future growth. In addition, Jasper

Avenue began to decline in the 1980s with the rise of the interior shopping mall, in particular the West Edmonton Mall. The mid-1980s saw the beginning of reinvestment in the downtown core and Jasper Avenue through a series of public investment initiatives, including the underground LRT in 1985 and the subsequent redesign of the Jasper Avenue streetscape. Slow reinvestment has continued into the 2000s; however, infrastructure and the streetscape quality of Jasper Avenue is dated, deteriorated and ill-equipped to support further growth. As Edmonton continues to grow and more development is focused to the downtown, the redesign and reimagining of Jasper Avenue has become a priority for the City.



1920s



1960s



1980's - West Edmonton Mall

## 2.2 Jasper Avenue's Existing Conditions: Issues and Challenges

*Today, approximately 75% of the Jasper Avenue's ROW is dedicated to vehicular needs. A rebalancing of the ROW to allocate more space to pedestrians and transit users is the first step in revitalizing the street.*

Many of the contemporary challenges facing Jasper Avenue are the result of 50 years of post-War growth. Positive change, reinvestment and development are needed to improve the urban character, vitality and commercial strength of Jasper Avenue. This will require more than a streetscape upgrade - a comprehensive strategy is needed to guide revitalization of the area based on a clear understanding of contemporary issues, challenges and opportunities on and around Jasper Avenue.

Early in the study process, the study team undertook an investigation of Jasper Avenue's existing conditions and explored its role within the downtown and the City as a whole. This section summarizes the key issues and challenges necessary to understand to inform recommendations contained in this study.

Jasper Avenue is a busy pedestrian corridor with approximately 2,000 pedestrian crossings at the 100th and 101st Street intersections during the morning rush hour. This is a very significant amount of pedestrian traffic which is confined to a relatively narrow sidewalk area typically 4.1 to 4.3 metres wide including the curbs. Jasper Avenue is generally a 30 to 30.5 metre ROW and has been configured as a 7-lane roadway, consisting of a dedicated left-turn lane, two travel lanes in each direction and curb side parallel parking also used for bus stops. The north and south curb lanes

accommodate metered parking, buses and bus stops, commercial loading zones, as well as right turning traffic near intersections. Approximately 22 metres of the ROW is dedicated to automobiles which leaves approximately two 4.1 to 4.3 metre wide pedestrian sidewalk or boulevard areas on either side of the street. In short, the current configuration of Jasper Avenue does not prioritize pedestrian use. With a seven lane configuration, almost 75% of Jasper Avenue's ROW is dedicated to traffic movement and just slightly more than 25% is available to pedestrians.

Best practice research associated with high performing commercial shopping streets reveals that to support vibrant pedestrian activity, no less than 40% of the ROW should be for pedestrian use, with about 60% dedicated to roadway. Whyte Avenue is an example of a vibrant street in the City of Edmonton that adheres to this 60/40% principle. Whyte Avenue has approximately 40% of its ROW dedicated to public realm including sidewalks and the center median. Unlike Jasper Avenue, Whyte Avenue's sidewalks have the additional advantage of not being encroached by the utility kiosks and LRT station entrances that clutter and reduce the useable sidewalk area. Further, Whyte Avenue's

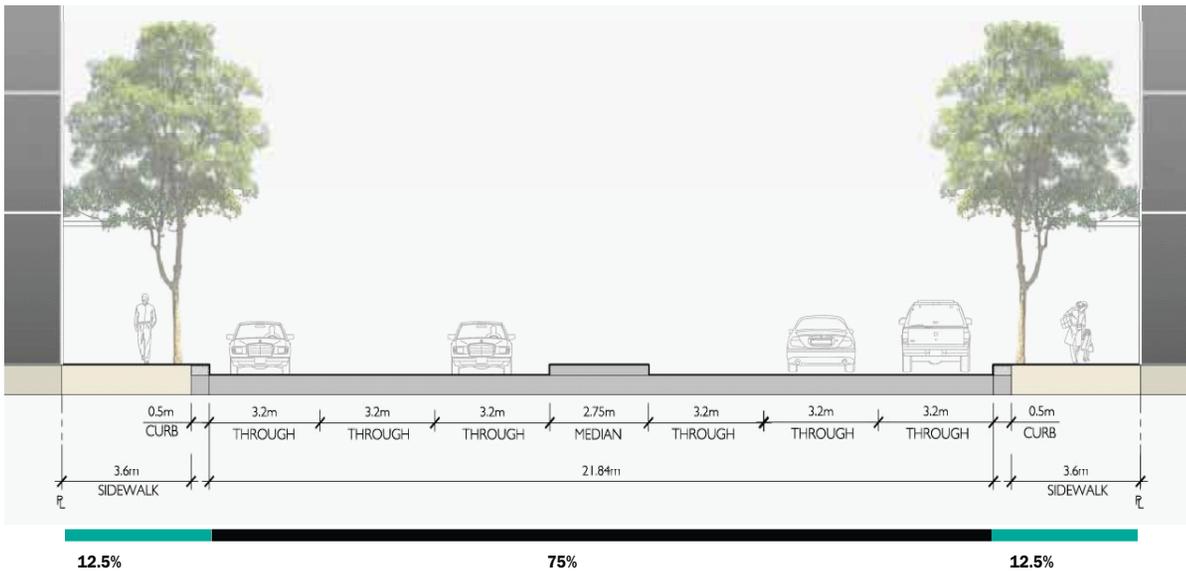
streetscape is in better condition, and better supports a vibrant shopping, dining and entertainment experience.

In addition to being a busy pedestrian corridor, Jasper Avenue is an important transit corridor within the downtown. Jasper Avenue handles over sixty buses during the peak hour and the LRT is situated below the street. The three LRT stations along Jasper Avenue within the study area handle a total of 18,000 passengers per day; Central Station is the busiest with over 8,400 passengers per day. There is also

a significant amount of bus transit crossing Jasper Avenue on 100<sup>th</sup> and 101<sup>st</sup> Streets creating a significant amount of transfer traffic between bus routes, as well as between buses and the LRT in the vicinity of Central Station. Two transit lay-bys exist along Jasper Avenue; an east-bound lay-by on the south side of Jasper Avenue, just east of 101<sup>st</sup> Street; and a westbound lay-by along the north side of Jasper Avenue just west of 101<sup>st</sup> Street. The significant amount of transit passenger activity at Central station correlates with the high pedestrian volumes at Jasper Avenue and 100<sup>th</sup> Street and Jasper Avenue and 101<sup>st</sup> Street.



▲ Whyte Avenue in the City of Edmonton



▲ Figure 2  
Existing Jasper Avenue Section

To better facilitate pedestrian and transit use along Jasper Avenue, there is a fundamental need to reconfigure the ROW to provide more space for pedestrian use. To achieve a better balance adhering to a 60/40 % ratio of roadway to pedestrian space, Jasper Avenue sidewalks should be widened to a minimum 6 metre width. This change would result in an improved pedestrian setting that is more appropriate to the high volumes of pedestrians and transit use on the street. It would also create the physical space, which would enable Jasper Avenue to again evolve into signature shopping destination, an attractive centre-piece for downtown and a catalyst for downtown city-building.

*Transportation capacity indicates that sidewalk widening on Jasper Avenue is feasible and is consistent with the strategic goals of the City's Transportation Master Plan.*

## Transportation Capacity

Jasper Avenue is a major access route to the downtown from both the east and the west. Traffic volumes along Jasper Avenue have remained fairly stable over the last ten years both on a daily basis as well as during peak hours. Jasper Avenue carries approximately 18,000 to 30,000 cars per day (500 to 1,250 automobiles per hour), with the highest volumes occurring at 109<sup>th</sup> Street. Weekday peak hours are 7:30am to 8:30am and again between 4:30pm and 5:30pm.

Jasper Avenue operates well during rush hours as traffic levels are well below the street's overall capacity. With a saturation flow rate of 1,750 vehicles per hour and an average green split of 55%, the lane capacity on Jasper Avenue is about 960 vehicles per hour. During the morning peak, nine of twelve intersections have through traffic that is below this capacity; two intersections are within 50 vehicles of this capacity and 109<sup>th</sup> Street is significantly above this value. During

the afternoon peak, eight of twelve intersections have through traffic at or below this capacity level, with three intersections having through traffic above this value. Traffic volumes are much lower at other times during the day, on the weekends, and lowest during the evening. In general, Jasper Avenue operates below or well below capacity 92% of the time.

Jasper Avenue is not a preferred or efficient route for through traffic movement due to the high number of traffic lights, curbside parking and bus stops which all tend to slow down traffic flow. Drivers prefer alternate routes such as 102<sup>nd</sup> Avenue and 104<sup>th</sup> Avenue to move through the downtown. The City of Edmonton's connected network of downtown streets provides users with a range of route choices and alternatives to Jasper Avenue.



▲ The existing streetscape along Jasper Avenue is dated and deteriorated.

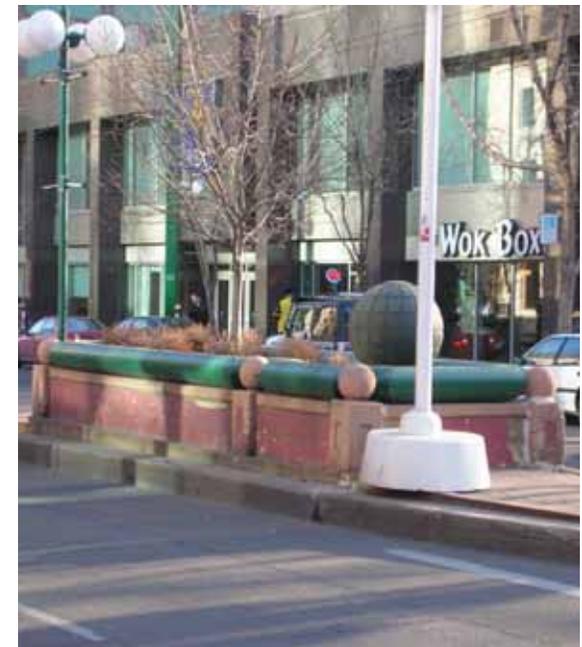
Given these factors and through detailed discussions with the City’s transit and transportation departments, it was determined that there is sufficient capacity to remove one through lane in each direction to accommodate significant sidewalk widening appropriate for a downtown main street and major transit corridor, without significant transportation impacts.

The overall downtown network has capacity to absorb resultant route diversions with some changes to intersection timing and minor improvements to parallel routes. The 7-lane roadway can be reconfigured successfully as a 5-lane roadway with all five lanes available to traffic during peak hours; two through lanes and one left turn lane. During off peak hours, the curb lanes would be available for parking, bus stops and commercial loading. A 4 metre wide curb lane can provide cyclists with ample space to travel with ease and safety. East and westbound transit lay-bys will need to be accommodated on either side of 101st Street to accommodate transit transfers.

The reconfiguration of Jasper Avenue is consistent with a number of goals outlined in the 2009 City of Edmonton Transportation Master Plan, which aimed at reducing the reliance on the automobile and encouraging public transportation and active transportation as preferred mobility choices. It serves as a reflection of the policy directions of the City’s CCDPlan and Transportation Master Plan and will reduce reliance on automobile use on an important transit corridor by managing demand on the downtown street system while providing opportunities to improve the viability of alternative modes of travel, including walking and cycling.

Street Segment	Volume (Number of Vehicles per hour)
97th Street to 109th Street	735 to 1,145
100th Street to 106th Street	735 to 800
108th Street to 109th Street	1,00 to 1,150

▲ Figure 10  
Jasper Avenue Traffic Volumes



▲ The central median prevents Jasper Avenue from being used as a major parade route for civic events

*The reconfiguration of Jasper Avenue as a pedestrian and transit- orientated main street presents a number of design and engineering challenges which must be addressed through the new streetscape concept.*

## Streetscape Improvements

Rebalancing Jasper Avenue's ROW to better prioritize pedestrian and transit use will bring a range of complicated design and engineering issues. LRT tunnel and station roof elevations and walls are fixed so new streetscape design must carefully consider these constraints to achieve inspiring yet practical solutions. While sidewalks can be widened and extended over LRT station locations, this presents grading challenges. Solutions need to be explored to ensure appropriate sidewalk drainage. Various other elements of the streetscape including light standards and trees may potentially not be located directly over the LRT tunnel. Air and fire vents associated with the LRT, some of which are accommodated in the central median between 101st and 102nd Streets, may be relocated, but need to be accommodated somewhere on the public boulevard for functional and safety reasons. Further, consideration of the design of LRT

station entrance points must be explored to enhance way-finding and the overall physical appearance and functionality of the Jasper Avenue streetscape.

The green utility kiosks on Jasper Avenue greatly contribute to the Avenue's dated and cluttered appearance, as well as detracts from pedestrian functionality, and reduce visibility and the perceived sense of safety. Removal of the kiosks and the reconfiguration and relocation of the utilities within a less visually and physically intrusive form either on the street or preferably along the side streets, will be a complex design and engineering consideration. Solutions will involve discussions within the City, as well as with the utility providers.



▲ Existing Utility Kiosk along Jasper Avenue

Consideration of material palette, durability, life cycle of materials and sustainability issues will also need to guide the streetscape design. Material selection should allow the streetscape to have an extended life span of 80 to 100 years before needing significant reinvestment and repair. It is interesting to note that Frederick Law Olmstead's and Calvert Vaux's Central Park in New York, which was designed in 1858, has only undergone one significant repair and restoration over the last 150 years. While this did include restoration of hard landscaping, the quality and longevity of the original materials, as well as the overall quality of construction largely enabled repair rather than replacement of structures that have been in place for a very long time. Long term sustainability and the reduction of operating expenses will be important criteria in developing the overall streetscape strategy for Jasper Avenue.

*The existing streetscape is dated, deteriorated and overly cluttered, detracting from the image of the street as a premier destination. It has reached the end of its lifecycle and is in need of replacement.*

As noted above, Jasper Avenue's last significant streetscape improvement occurred in the mid 1980s. This effort was aimed at bolstering the street as a vital, attractive and unique place in response to significant suburban commercial expansion, which pulled people and shopping activity away from the downtown. Approximately 25 years later, Jasper Avenue's streetscape appears dated and deteriorated. Specifically, very substantive traffic signal standards and utility kiosks appear as oversized, heavy objects that clutter the streetscape and obscure views down the street and into shop windows. The location and placement of LRT station access points compresses walking areas between building fronts and the station edge to as little as 2 to 2.5 metres. The haphazard placement of street furniture, including benches, lighting standards, garbage receptacles and newspaper boxes further reduces the walkable sidewalk area leading to meandering and constrained pedestrian movement along the Avenue. Standard-size brick pavers have been heaved and dislodged as a result of

The existing streetscape along Jasper Avenue is cluttered and in need of repair

harsh winters and are out of scale with the large scale buildings and street in which they are situated. Finally, inconsistent maintenance and patches of differing materials from various repairs add to the deteriorated condition of the streetscape.

The streetscape has largely reached the end of its useful lifecycle and is in need of regeneration to improve pedestrian movement and functionality, as well as bring a more contemporary and renewed sense of place to Jasper Avenue.



*Ground floor uses do not always enliven and activate the public realm and the street lacks a distinct or definable commercial role.*

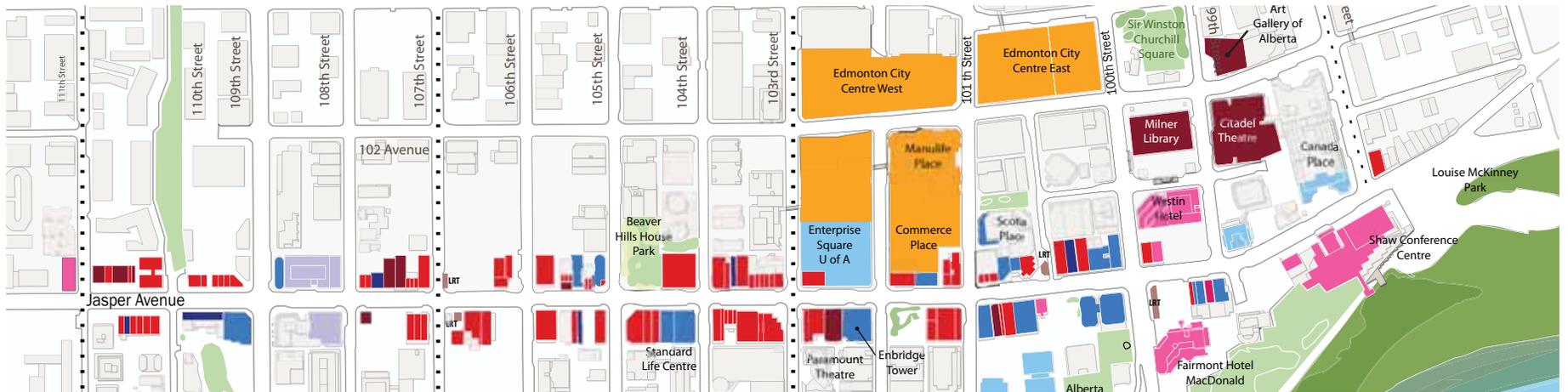
## Street Animation

As a main street, Jasper Avenue is not consistent along its length and does not have a well defined commercial role. In examining the uses along the street, Jasper Avenue is comprised of distinct street segments, each with differing use characteristics and development potential. East of 103rd Street on Jasper Avenue, corporate offices and institutional uses account for approximately 50% of the development frontage to 100th Street. As a result, there is less opportunity to create a continuous high value retail main street environment. Jasper Avenue west of 103rd Street has a more mixed use and retail character and includes important retail amenities such as Sobeys and well-designed historic main street buildings. The area west of 103rd Street also has more redevelopment opportunities, including the future introduction of well designed new commercial and mixed use buildings. This portion of the street has the potential to create

a critical mass of retail that will draw more people to Jasper Avenue. The western portion of the study area also includes various entertainment establishments on and around 109th Street, which require improvement.

As a whole, ground floor uses lining Jasper Avenue have a major influence on the vitality and drawing power of the street in the downtown and within the City. Currently, there are a number of areas where ground floor uses are active and capable of generating foot traffic. These include restaurants and retail stores, however, many are isolated from one another and do not create a distinct retail or commercial cluster on the street. Banks, large office lobbies and surface parking lots create significant 'dead zones' which detract from the commercial continuity of the street.

In addition to the fragmented nature of commercial activity on the street, major shopping in the downtown has gravitated to indoor shopping centers, located just north of the Jasper Avenue. Buying power is today focused to these indoor centres and not on Jasper Avenue. Even the former Bay store previously located on Jasper Avenue has relocated to the indoor shopping environment. There are a range of strategies that can reinforce the commercial viability and draw of the Jasper Avenue. The visual and physical quality of the street must be enhanced, commercial uses at grade must be required through policy and targeted actions, and an appropriate merchandizing strategy must be developed for differing segments of the street.



▲ Figure 11  
Jasper Avenue Street Wall and Commercial Fabric

- |                                   |                     |                      |
|-----------------------------------|---------------------|----------------------|
| ■ Civic & Cultural Use            | ■ Institutional Use | ■ Under Construction |
| ■ Ground floor below street level | ■ Office Lobby      | ■ Retail             |
| ■ Hospitality                     | ■ Interior Malls    |                      |



*There are gaps in the Jasper Avenue street wall and commercial fabric. These voids need to be filled with high quality urban buildings to improve the main street and pedestrian experience.*

In several sections of Jasper Avenue, the street wall is well defined and creates a high level of visual interest, connecting indoor activities with the outdoor walking experience. However, other areas possess significant gaps in the street wall. These include parking lots and undeveloped lands, blank facades, or shop fronts which are raised above or below the walking surface. In some instances, new development has filled these gaps, but the form, scale and relationship of some developments are not appropriate for Jasper Avenue, nor do they support the vitality of the main street. For example, the one storey commercial development at 109th street, which includes the Save on Foods grocery store, does include a mix of commercial uses, but many of these uses turn their backs to Jasper Avenue and have their primary entrances internal to the block, facing the large surface parking lot.

In this location, the Jasper Avenue frontage is comprised of signage, dark tinted windows, and a retaining wall which elevates and further separates this development from Jasper Avenue. This type of suburban style development is inappropriate for the centre of downtown. It is out of context in terms of the scale and height of other adjacent developments, and most importantly, it is a single purpose commercial use, meaning it represents a missed opportunity to include and integrate residential or office uses above the ground floor as part of the overall development. More complex mixed use elements could add people, vitality and an increased tax base to the downtown. The new Sobeys at 104th Street is an excellent example of a development that adds complexity and quality to the downtown and is supportive of Jasper Avenue and the desired level of activity on the street.

City building, particularly in a downtown, is a long term activity that requires well understood objectives to coordinate the actions of many players. Because the results of city building will be in place for generations, it is important to get the 'vision' and goals right. To this end, gaps in the Jasper Avenue street wall, including places where buildings are absent or where low scale buildings are 'underperforming' should be filled with high quality mixed use developments, which will animate Jasper Avenue and complement the downtown as a whole.

One of several vacant or underutilized parcels of land fronting Jasper Avenue. ►



*Not enough people live on Jasper Avenue or in the defined Downtown boundary area and as a result there is a lack of amenities, which are needed to attract downtown residents and create a ‘liveable’ downtown.*

### The Jasper Avenue Neighbourhood

One of the fundamental characteristics of a good shopping street is its connections to its supporting residential neighbourhoods. Typically, this means that the street immediately abuts the supporting neighbourhood, allowing for a high level of pedestrian traffic and visitation. While Edmonton’s downtown population has experienced positive growth over the past decade, including a doubling of its residential population to 11,000 people, the population living within the defined boundaries of the downtown is still relatively low. In sharp contrast, the downtown is home to approximately 78,000 jobs. This means that downtown is largely a place to work, with the vast majority of people traveling to and from the downtown each day. With a modal share of only 12% for public transit use, with the majority of people coming and going by automobile.

Perhaps the single most important factor in rejuvenating the shopping role of Jasper Avenue, and enlivening the downtown as a whole, is to add a much greater residential population. The downtown needs to evolve as neighbourhood which can be home to a broad cross section of people from singles and retirees, to students and families. It must become a complete community. The overwhelming trend across Canadian cities is to transform traditional Central Business Districts into more complete downtown communities by accelerating residential growth. Cities such as Vancouver and Toronto have added tens of thousands of residents to their downtowns, improving the balance between the number of residents and jobs within their downtowns. Pursuing this same strategy for downtown Edmonton would have a significant impact on the economic, civic and cultural vitality of the downtown. An improved live/work ratio would also create demand for additional retail, business and personal services, which could be carefully targeted to Jasper Avenue.

A sustained program of downtown residential development is needed to support the positive evolution of Jasper Avenue and elevate downtown Edmonton as one of Canada’s great urban neighborhoods. To be successful, and attract a diverse population to the downtown, a range of housing choices are needed. While this will generally be high density housing, grade related housing and a range of two the three bedroom units are essential to enable families to live in the downtown. At the same time, new population must be supported by a range of other community service and recreational amenities. This includes consideration of downtown schools, libraries, and recreational facilities as well as a significant amount of new park lands.

Beaver Hills House Park, one of four parks in the study area



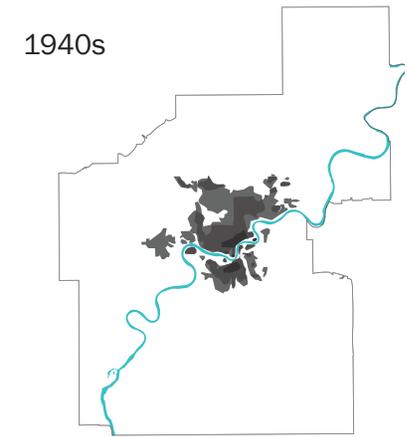
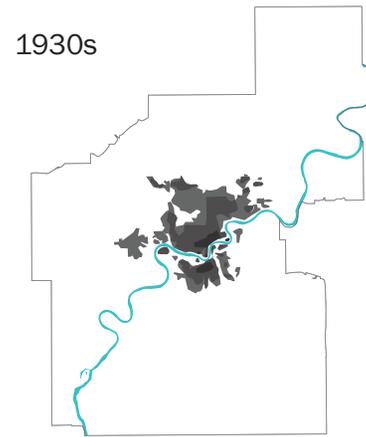
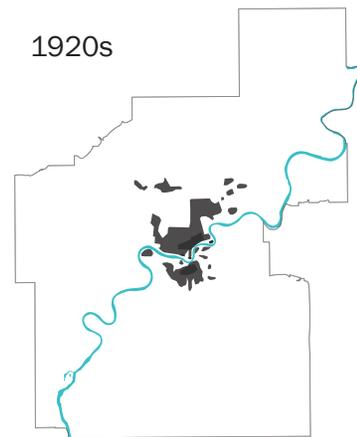
## Parks and Open Spaces

While the downtown is set against the remarkable natural amenity of the North Saskatchewan River Valley, which provides a range of recreational trail and park amenities, the downtown itself has very few parks and open space. Within the Jasper Avenue study area, there are only four public park areas. These include The Railtown Park, Beaver Hills House Park, Beatrice Carmichael Park, and Veteran's Square. While there are other open spaces along Jasper Avenue, including the open space in front of Hotel MacDonald, these are privately owned and can be developed at any time. The total park space within the study area is only 2.6 hectares, which is approximately 3% of the land area within the overall Jasper Avenue study area.

In contemplating significant residential development in the downtown, a corresponding increase in dedicated park land is needed. Cities with vibrant downtowns and large residential populations all have extensive downtown park systems. The Cities of Boston, New York, San Francisco and Vancouver all have highly developed park systems, which provide urban dwellers relief and the ability to be in touch with nature in dense built environments. These places provide residents with a range of leisure and recreational opportunities that enhance quality of life and contribute to tourism. An expanded and distinctive network of high quality parks, streetscapes and urban gathering places is needed in downtown Edmonton to support its evolution as a high quality living and working environment and to provide an attractive setting to host a variety civic and cultural events. Jasper Avenue should become a green main street spine, providing access and connection between places.

Examples of interesting parks and open spaces ►





*Edmonton will grow by 400,000 people by 2040; reconsidering existing development patterns and attracting just 6% of that growth to downtown over the next 30 years would translate into 24,000 new residents and generate approximately 75 - 20 storey residential buildings.*

### Targeting Growth to Jasper Avenue

Edmonton's post-war growth pattern is typical of North American cities; low density suburban development with an emphasis on automobile-oriented movement patterns. This pattern has had a significant impact on Jasper Avenue. Most notably, it has undermined Jasper Avenue's role as the centre of commercial, civic and cultural life in the City. The downtown remains a significant employment hub and offers the highest paying jobs in the region and yet the vast majority of new residential development continues to occur at the edges of the city.

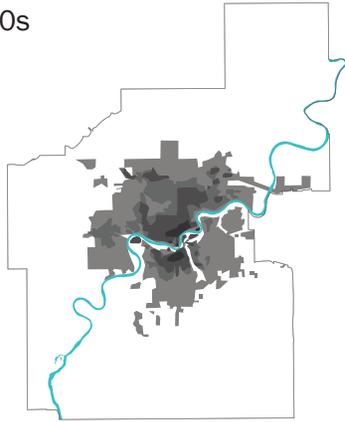
The effects of sprawl are well known. Rapid outward growth is increasingly being identified by cities as unsustainable. The cost of developing infrastructure, such as roads and transit and delivering services to low density, inefficiently designed communities is putting a significant strain on municipal resources. As such, cities around North America are enacting plans and policies to grow more responsibly and reinvest in communities where infrastructure is already in place. For instance, the Province of Ontario has mandated that 40% of new growth within the Greater Golden Horseshoe Area, a region currently home to 8 million and expected to grow to 12 million by 2031, will occur within existing built areas. Further, greenfield development areas are required to meet minimum job and people density targets to reduce land consumption and support investment in transit infrastructure. Other municipalities across Canada are

also completing Integrated Community Sustainability Plans, demonstrating how they will achieve a range of sustainability goals. New policy directions and priorities are emphasizing inward-looking growth patterns that support compact mixed use development, transit and active transportation, environmental preservation, more efficient use of land, and urban revitalization.

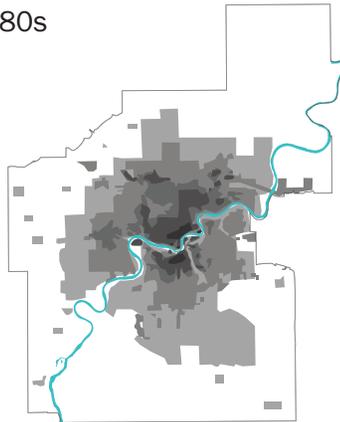
With the City of Edmonton's new and emerging policy frameworks, including the Alberta Land Use Framework, Capital Region Plan, Municipal Development Plan, Transportation Master Plan, and Capital City Downtown Plan support directions for more sustainable growth and prioritizing downtown revitalization.

Recent additions to the downtown, such as the new Sobeys grocery store and YMCA building, provide

1960s

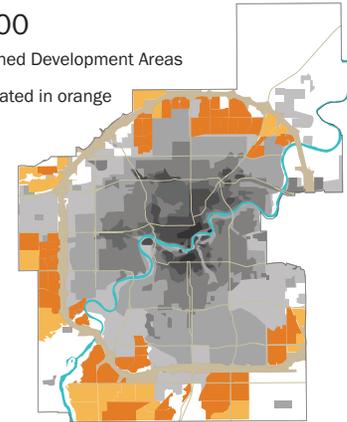


1980s



2000

Planned Development Areas  
indicated in orange



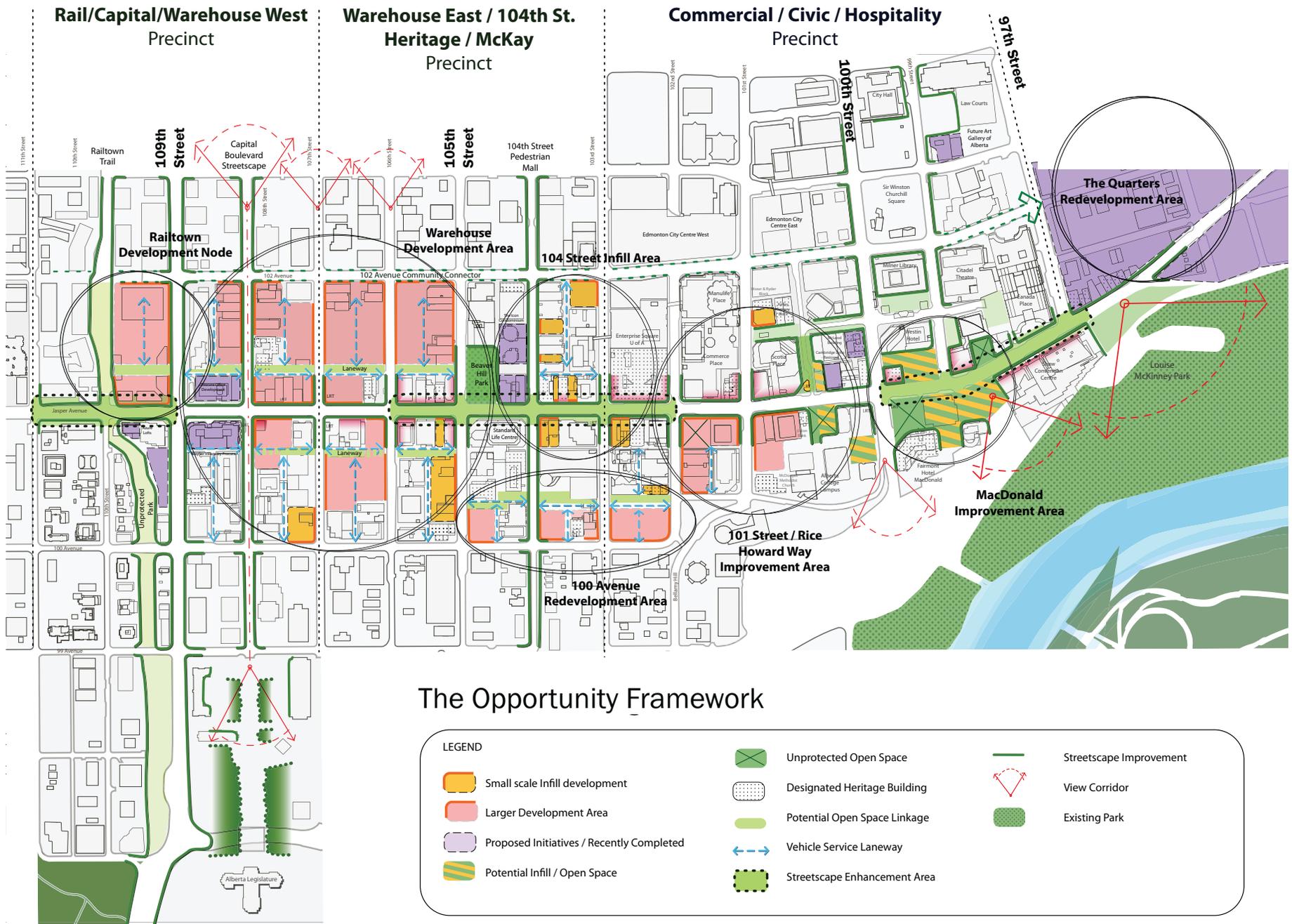
▲ Figure 13  
Growth of the City of Edmonton

▼ The City of Vancouver has focused on intensifying the downtown with high quality mixed use developments to achieve urban vitality and great city building

important amenities that help to make the downtown a much more livable place enabling it to more easily attract downtown residents. New developments such as the Icon towers have been eagerly awaited by a growing segment of the community who want to live in the downtown and enjoy an urban lifestyle.

This includes not only young professionals, but also the growing numbers of post secondary students in central Edmonton that are looking to reside in a downtown environment. These demographic groups, together with an aging population demographic that is looking to downsize close to amenities, creates a strong market rationale for downtown housing development. Reimagining Jasper Avenue is the first step in creating an improved physical setting to support residential, office and commercial growth in the downtown and create a thriving downtown community.





▲ Figure 14  
Jasper Avenue's Opportunity Framework

## 2.3 Jasper Avenue's Opportunity Framework

*The Jasper Avenue corridor has great potential for significant change, reinvestment and redevelopment. City building within the corridor needs to be guided by careful planning, a clear vision for the future and a range of targeted actions to achieve successful revitalization.*

Within the study area, large quantities of land are occupied by surface parking and lower scale structures, which do not have an established heritage value or character. These underutilized lands represent prime redevelopment opportunities both on and adjacent to Jasper Avenue. As identified in Figure 14, in the western portion of the study area, north of Jasper Avenue, there are large contiguous land parcels that present important opportunities to combine major redevelopment with new open spaces. Careful planning and design of these areas could significantly improve the overall image, character and amenity of the downtown, resulting in an alternative high density downtown neighbourhood.

Within the eastern portion of the study area, a number of smaller vacant or underutilized lands, including privately held open spaces, present additional infill opportunities. Clear urban design direction is needed to guide redevelopment on these infill sites to ensure they positively contribute to the character of the corridor.

The Opportunity Framework identifies major redevelopment clusters and infill sites within the study area. It illustrates the fact that approximately 40% of the land within the Jasper Avenue study corridor has real potential for redevelopment over the next 20 to 30

years. These clustered groups of redevelopment areas should be cohesively and comprehensively planned to ensure that the urban character, use and relationships to the adjacent context will bring a positive contribution to the whole of the downtown.

Investment in a new streetscape for Jasper Avenue is the first step in improving the character and function of the corridor. Through the creation of a clear vision and enforcement of appropriate urban design guidelines, investment along Jasper Avenue can be leveraged into significant city building activities. Effective implementation can ensure a significant return on the initial investment in Jasper Avenue's streetscape including:

- a renewed vitality and pride in downtown;
- an improved sense of place and character within downtown;
- an enhanced sense of personal safety;
- cultural growth;
- an improved social cohesion;
- an increased interest in downtown living;
- private sector redevelopment; and,
- increased tax revenue.

Today, there is significant cultural momentum in downtown Edmonton; city building within the Jasper Avenue corridor will enable the Avenue to play a greater role in Edmonton's cultural development. Downtown Edmonton is fortunate to have a number of cultural institutions that contribute to the vitality of the core and to the quality of life in the City. The Winspear Centre, Citadel Theatre and Art Gallery of Alberta represent the kinds of important cultural destinations which inject energy and vibrancy to the downtown core. Recent additions such University of Alberta Enterprise Centre on Jasper Avenue and plans for the expansion of the Grant MacEwan University campus and Norquest College, add an important youthful student population to the downtown. The renewal of Jasper Avenue will help build more cultural momentum by allowing it to host important cultural and civic events, parades and festivals. Jasper Avenue also provides a physical connection between many of the important educational and cultural institutions in the downtown and a range of public gathering spaces.

Historic buildings on Jasper Avenue, which add to a sense of place along the corridor, should be protected as part of the renewal of downtown.



▲ The Jasper Avenue New Vision initiative hosted a three day Ideas Workshop

## 2.4 Ideas Charrette: What we heard from the community

*By 2030...Jasper Avenue has been transformed into Edmonton's signature main street in Alberta's Capital City. It is a place that reflects the vibrancy, authenticity, diversity and aspirations for all Edmontonians.*

As part of the community visioning process for the Jasper Avenue New Vision Initiative, the City and consultants carried out a focused three day Ideas Workshop to generate ideas and design concepts for the study area and establish an early conceptual vision for the Jasper Avenue's Opportunity Framework.

The event brought together a broad array of stakeholders, including the general public, representatives of downtown institutions, such as the University of Alberta, land-owners, developers, business owners, institutions, representatives from the Province, various municipal departments, and a large contingent of design professionals including architects and landscape architects. Through a series of presentations and breakout group assignments, participants were asked to formulate preliminary principles and design propositions for the Jasper Avenue streetscape and Opportunity Framework.

The principles and design propositions were then represented on a large scale model depicting the study area in the context of the larger downtown. This intensive consultation and working event generated the major urban design, public realm and streetscape concepts which have subsequently been refined and detailed and presented at additional public consultation events.

Over the course of the three day workshop, participants explored a range of propositions for the massing of new development, new park additions and additional streetscape improvements. This iterative process was stimulating, educational and created opportunities for discussion and debate amongst participants concerning design concepts, appropriate built form, massing and height solutions.

Participants began the workshop by identifying a wide range of aspirations for Jasper Avenue, as well as what they thought the greatest challenges would be to overcome. Through discussion and review, participants used these aspirations and challenges to develop and model a vision concept for the streetscape and study area corridor.

## Participants' Aspirations for Jasper Avenue

Place People Friendly Vibrant Civic  
Imagination A Monument Interesting  
Narrative Green Transparent Poetic Civic  
Commitment Public Art Musical Relevant  
to Residents Exciting Celebration Simple  
Bold Elegant Visionary Provocative and  
Compelling Winter Attitude Walkable  
Private Sector Partners Sustainable Civic  
Pride Fun Identity Profitable Dare to Dream  
Comfortable and Inviting Unique Beautiful

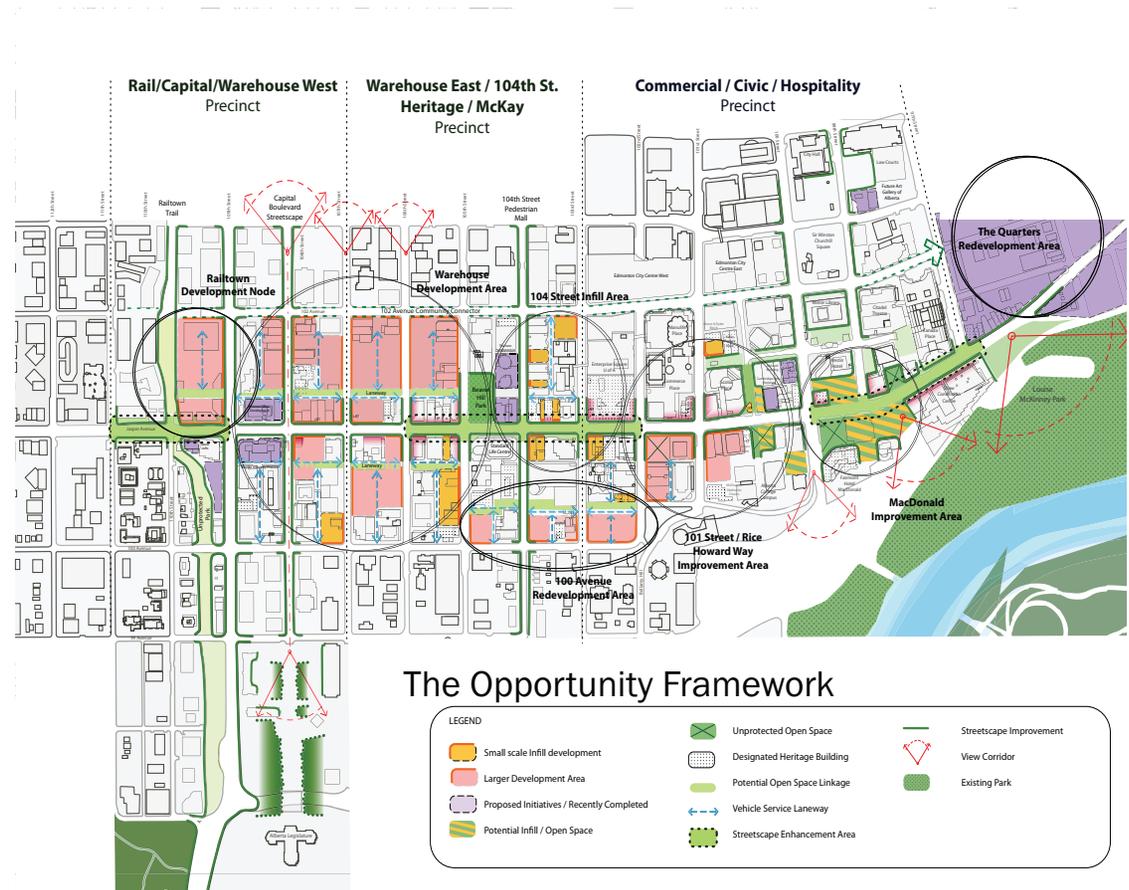
## Participants' Thoughts on Key Challenges to Overcome

Temptation to Cheap Out Mediocrity Transportation  
Department Surface Parking – Getting Rid of It Fast Traffic  
Slow Growth Existing Mindset Political Compromise  
Commitment to Change Winter Maintenance Long Term  
Adaptability Political Will Retail Market Citizen Buy  
In Fear of Taking Risks Street Level Safety Inability  
to Dream Lack of Density Weather Lack of Vision  
Incentives for Businesses to Activate Street Long Term  
and Stable Funding Competition from Big Box and  
Regional Shopping Centres Follow Through on Ideas  
Complacency Balancing Transit, Cars and Pedestrians  
Scale – Keeping with Existing Character Has to be  
Politically, Economically and Developmentally Doable

## Idea Workshop Themes and Conceptual Design Outcomes

Through the Ideas Workshop a number of themes and potential design strategies were identified for Jasper Avenue, including:

- Rebalance the public right of way to provide more space for pedestrians by reducing the street from 7 to 5-lanes and widen sidewalks to 6 metres;
- Consider a 4 block street segment between 102nd and 106th Street narrowed to 4-lanes to allow for a 9 metre wide outdoor dining and retail shopping area on the north side of the street;
- Consider gateway moments at each end of the site area at 97th Street and 110th Street which may accommodate a central planted median;
- Employ straight curb lines instead of curb bump outs and parking lay-bys;
- Redevelop underutilized land and reconfigure existing buildings fronting Jasper Avenue to become more engaging and contribute to an active retail-oriented signature street;
- Build up surrounding neighbourhoods with a critical mass of new residents to support and champion continued positive change in the downtown;
- Improve existing open spaces including Beaver Hill Park and the rail corridor and create significant new open space opportunities; and,
- Design a cleaner, simpler, greener, more durable, functional and beautiful streetscape.



▲ Figure 15  
Jasper Avenue's Opportunity Framework

## Precincts:



### **Precinct Area 1: Commercial, Hospitality and Civic Centre (Blocks 97-103)**

- Establish a eastern gateway element with possible planted median between 97<sup>th</sup> and 99<sup>th</sup> Streets;
- Secure public open space in front of Hotel Macdonald;
- Capture opportunities for views and improved access to the river valley edge;
- Improve 99<sup>th</sup> and 100<sup>th</sup> Street connections into the civic and cultural precinct and Churchill Square through future streetscape improvements;
- Employ a 5-lane roadway with wider sidewalks;
- Infill remaining development sites with mixed use and office development;
- Encourage office buildings to retrofit ground floors with street-oriented retail and restaurants; and,
- Encourage the retrofit of blank and bland building facades and minimize the size of office lobby uses at-grade.



### **Precinct Area 2: Warehouse East/104<sup>th</sup> Street Heritage District/ McKay Neighbourhood\***

- Consider a 4-lane roadway between 102<sup>nd</sup> and 106<sup>th</sup> Streets to achieve 9 metre wide sidewalk;
- Incorporate a double row of trees where possible;
- Re-imagine Beaver Hill Park as an urban square at the heart of Jasper Avenue;
- Line portions of Beaver Hills House Park with retail uses;
- Extend the Beaver Hills House Park plaza across adjacent streets by extending paving materials;
- Take advantage of significant development potential, including establishing a new high density urban neighbourhoods in the Warehouse District to support Jasper Avenue;
- Create a significant new park connecting Beaver Hills Park within new neighbourhood development; and,
- Encourage buildings retrofits or upgrades to building facades in order to better contribute to continuous retail and pedestrian-supportive design.



### **Precinct Area 3: Railtown/Capital Boulevard Warehouse West\***

- Consider creating a Western Gateway feature with the reconstructed 109<sup>th</sup> Street Bridge connecting Rail trails on the north and south sides of Jasper Avenue at 110<sup>th</sup> Street;
- Create a planted central median;
- Create 5-lanes of roadway with widened sidewalks;
- Redevelop underutilized and vacant land with significant mixed use and residential development;
- Tie together the significant railway corridor open space network through improved connections; and,
- Create a strong connection with the Capital Boulevard and Capital Precinct through improved streetscaping.

\* Precinct 2 and 3 model images are conceptual, and are for illustrative purposes only.



# Part Three: Jasper Avenue New Vision- A Framework for Renewal, Reinvestment and Focused City Building

*The section of the report outlines Jasper Avenue New Vision- a framework for renewal, reinvestment and focused city building that will reposition and strengthen Jasper Avenue as Edmonton’s signature main street. It articulates a broadly accepted vision for the street, the streetscape and the surrounding physical context, initiated at the Ideas Workshop and subsequently refined by the City and consultant team. It builds carefully on existing assets and opportunities and creates a range of incremental city building moves to encourage and guide desired reinvestment and development in this part of Downtown Edmonton.*

## Legend

### Vision Elements

- proposed new development (high rise)
- proposed new development (low rise)
- proposed new public space

### Existing Urban Fabric

- commercial/office mixed use (high rise)
- commercial/office mixed use (low rise)
- civic/institutional
- residential (high density, high rise)
- residential (high density, low rise)
- residential (low density)
- commercial

Figure 16  
New Vision for Jasper Avenue

## 3.1 Components of the Vision Framework

*The New Vision positions the renewal of the streetscape as a catalyst for ongoing reinvestment in the Jasper Avenue corridor.*

The New Vision Framework creates a physical representation of how Jasper Avenue streetscape and corridor could evolve over the next 25 years. It aims at transforming Jasper Avenue into a corridor that supports a dynamic shopping experience and brings a renewed vitality and high quality developments to the downtown, creating opportunities for people to live, work and recreate. It creates a set of ambitions and aspirations about what the future of Jasper Avenue should be, and it outlines the recommended roles and responsibilities of the public sector in bringing forward the vision.

The New Vision places an emphasis on the quality of the public realm, the importance of continued public realm improvements and outlines these as important vision projects and/or strategies which should be prioritized and undertaken over the next 25 years. The vision also supports the creation of high quality built environments on private lands and establishes a set of policy and urban design recommendations, which should be utilized by the private sector to achieve a high standard of city building. These recommendations should also be used by the public sector in evaluating

the appropriateness of different propositions. Policy and urban design recommendations are presented in the next chapter of this document and include potential strategies for addressing mobility, land use, and urban design issues. These recommendations should be reviewed and integrated within the CCDPlan and Zoning By-law to achieve the goals of this study and a more vibrant, active and attractive Jasper Avenue and downtown environment.

The components of the vision framework include:

**Planning and Design Principles** which provide a foundation for good city building and will be used to guide development, investment and decision making to improve the Jasper Avenue corridor.

The **New Vision** imagines the study area 25 years into the future and translates planning and design principles, as well as municipal and broad community goals into a physical vision for the area. The Vision demonstrates how development can be managed to achieve a range of new opportunities for living, working and recreating.

**Six Big Moves** describe the major thrusts of the Vision and the extent of change for major development areas in the study area. The vision makes recommendations for a range of additional public realm improvement strategies which should be prioritized and undertaken over time.

The **Six Big Moves** include:

1. Re-vision Jasper Avenue as a catalyst for continued downtown development and re-investment.
2. Create a permanent public park in front of Hotel Macdonald and reinforce connections between Jasper Avenue, the riverfront and the civic district through a range of Civic Precinct public realm recommendations.
3. Complete the Heritage Trail along the river valley edge and create a new pedestrian mews linking Beatrice Carmichael Park to Veterans Park.
4. Promote intensification of the Capital District as a place for new mixed use and office developments.



5. Utilize the largest underutilized and vacant redevelopment sites to make a big impact downtown: create a new high density warehouse community and new central park just north of Jasper Avenue between Beaver Hills House Park and 108<sup>th</sup> Street.
6. Promote the continued intensification of the Railtown Community as a complete urban neighbourhood.

The New Vision includes a streetscape concept to guide the reconstruction of Jasper Avenue, which will result in significant improvements to the physical quality and character of the street. The renewal of the streetscape will create a needed catalyst for continued city building initiatives and reinvestment in the area.



▲ Figure 17  
Existing and Proposed Conditions for Jasper Avenue

## 3.2 Planning & Design Principles: A Foundation for Good City Building

*The following principles encapsulate the fundamental ideas that should guide decision making and help achieve the New Vision for Jasper Avenue.*



### **1. Leverage the Jasper Avenue streetscape initiative as a catalyst for downtown rejuvenation.**

Municipal commitment to the renewal of the Jasper Avenue streetscape is the first step toward a sustained program of revitalizing the downtown. City leadership will build interest and momentum and set a positive example of design excellence to be achieved in the downtown.



### **2. Create an attractive, high quality, distinctive streetscape which prioritizes pedestrian, transit and civic use throughout the year.**

A high quality public environment will better attract people, activity and investment. The new streetscape will be a place where pedestrians and transit users feel comfortable, safe and welcome.



### **3. More people need to live on Jasper Avenue to support its main street commercial functions and downtown vitality beyond the work day.**

The introduction of a significant permanent Jasper Avenue neighbourhood population will revitalize the commercial offerings of the street and significantly improve the overall safety and vitality of the area. While the trend toward downtown living is evident in recent projects, significant opportunities exist within the corridor for mixed use and residential intensification which must be a municipal priority.



### **4. Focus on place making – not projects.**

Every development opportunity should positively contribute to the overall quality, urban character and sense of place within the corridor. Evaluate development propositions on their ability to contribute to the overall quality of place.



## 5. Create a connected network of new and improved park spaces and streetscapes.

The Jasper Avenue corridor presents many opportunities to expand downtown's total open space resources linking people and activity along Jasper Avenue. It also creates an appropriate setting to attract people and investment.



## 6. Expand and diversify the commercial draw of Jasper Avenue

On main streets, such as Jasper Avenue, retail and commercial uses are the primary attractions for people. Mixed use development, the retrofit of existing buildings and a range of actions by the City and Downtown Business Association can guide improvement the retail offering on Jasper Avenue.



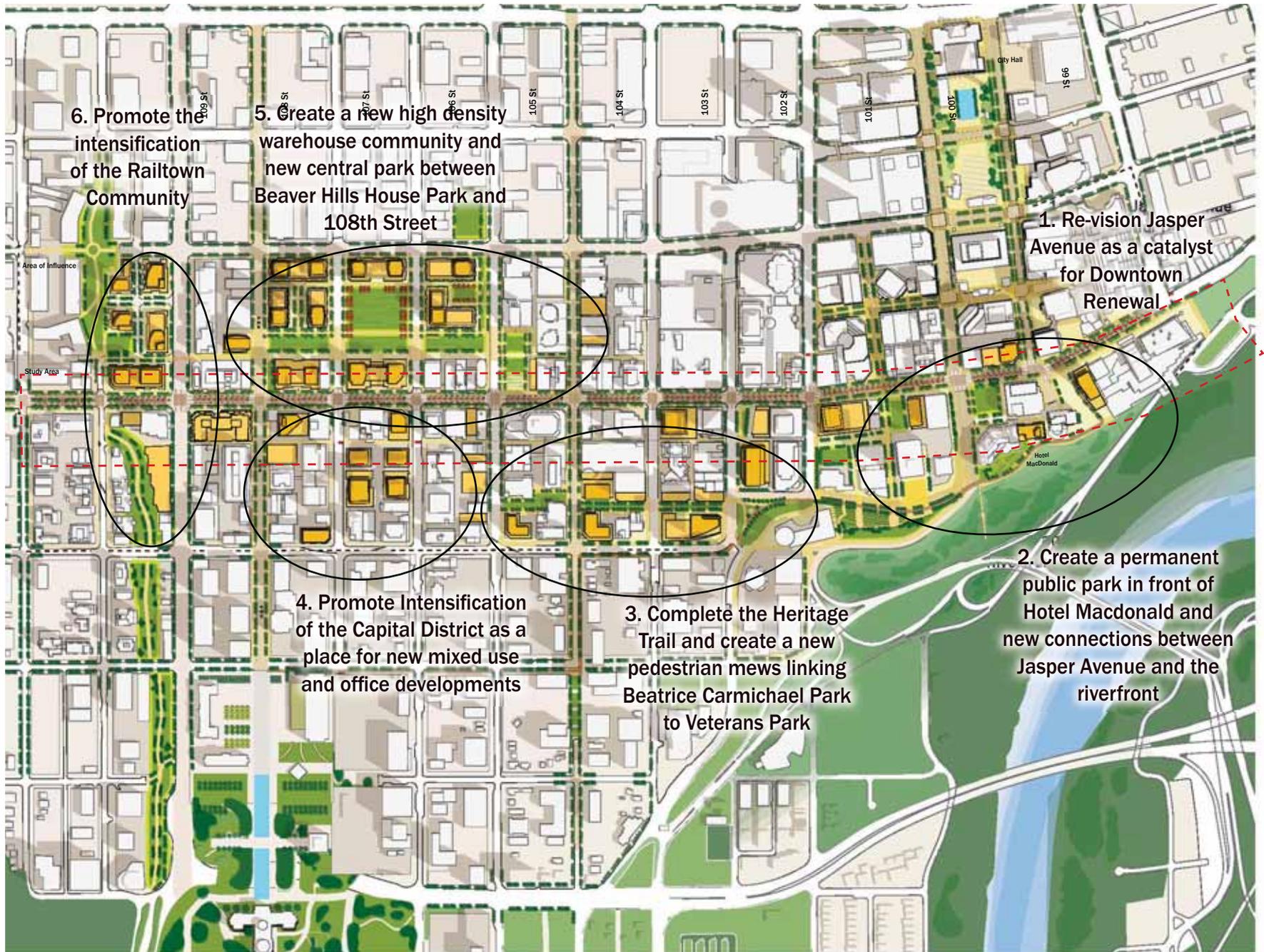
## 7. Protect and strengthen core assets along the Avenue

Jasper Avenue has a range of unique assets including heritage buildings and open spaces that positively contribute to the downtown. Protecting and enhancing these assets will strengthen the unique identity of the area and provide important anchors for further revitalization.



## 8. Strive towards a high quality of design excellence

A consistently high standard of design in streetscapes, parks and buildings is a critical factor in rejuvenating Jasper Avenue. A high standard of design established by the City through the streetscape renewal and specific urban design guidelines for the area will ensure the built environment will evolve in a manner appropriate to Jasper Avenue's important status.



▲ Figure 18

Six Big Moves for the Revitalization of Jasper Avenue

### 3.3 The New Vision: Looking 25 Years Ahead

#### The Purpose of the Vision

Over the course of the Jasper Avenue New Vision consultation process, stakeholders expressed a strong desire to see Jasper Avenue re-emerge as Edmonton's signature downtown destination. Over the last decade, progress has been made in the form of new high rise residential and office buildings as well as new pedestrian-oriented retail offerings.

Today, the corridor contains a wide range of commercial and community amenities, office uses, heritage resources, parks, and well loved retail destinations, which serve the City as a whole. However, there are many exciting opportunities for change that can successfully build on recent positive additions taking place in the downtown.

In today's knowledge-based economy, quality of place is becoming one of the primary factors that determine where a person chooses to live. As noted by Richard Florida in *The Rise of the Creative Class*, "competitive advantage has shifted to those cities and regions that can attract and retain the best talent... quality of place matters". Successful achievement of the New Vision can improve the City's overall competitive advantage and bring a renewed sense of pride and accomplishment in a more dynamic and vibrant downtown for the citizens of Edmonton.

#### Guiding Development

The New Vision is based upon an understanding of contemporary opportunities and challenges facing the area and the application of agreed to planning principles. As noted above, along with supporting policy and urban design recommendations presented in Part 4 of this document, the New Vision will be used to guide and coordinate public sector actions within the study area, and evaluate private sector proposals. To this end, the redevelopment concepts shown are suggestions aimed at illustrating opportunities and demonstrating the application of urban design recommendations. They do not bind individual property owners to the specifics shown.

When realized, the New Vision has the potential to bring:

- approximately 15,000 new residents;
- over 92,900 square metres of new office space;
- 60,387 square metres of new retail;
- 3.7 hectares of new parks space;
- \$19 million (2010 dollars) in new tax revenue;
- a range of enhancements to existing parks and streetscapes enhancing the attractiveness of the downtown;
- new mixed use and high density residential communities on and adjacent to Jasper Avenue; and,
- High quality public realm and built form demonstrating excellence in downtown city building.

#### Improving the Public Realm

Today, the New Vision study area is underserved by parks. A healthy urban environment is generally comprised of approximately 65% development sites; 20% streets and service lanes; and 15% open space including 10% public parks. Currently, parks in the study comprise only 3% of the gross land base. Other small privately held open spaces are not protected as municipal parks and may be redeveloped in the future. As new residents move into the area, a much more robust park system, as well as active public spaces, is needed to make the downtown livable and desirable.

A range of public realm initiatives are described in the Jasper Avenue New Vision, including the acquisition and development of new parks as well as enhancements and improvements to existing parks and streetscapes. This new public realm system will:

- create important public amenities;
- enhance land value;
- attract new development;
- support a broader range of recreational, civic, cultural and leisure opportunities;
- create a unique character and sense of place for different areas of the downtown;
- improve the quality of life for existing and future residents and workers; and,
- demonstrate the City's leadership and commitment to the ongoing process of building a much more livable downtown.

## 3.4 Six Big Moves in the New Vision

### Big Move #1: Re-vision Jasper Avenue as a catalyst for continued downtown development and re-investment

A renewed Jasper Avenue has the ability to be a strong catalyst for ongoing downtown reinvestment. The streetscape will create an attractive pedestrian-friendly environment that will set the stage for continued public and private sector development initiatives on and around the Avenue.

The rebuilding of Jasper Avenue's streetscape will occur incrementally between 97<sup>th</sup> Street to the east and 111<sup>th</sup> Street in the west. The redevelopment of the streetscape will include:

- a 5-lane roadway to achieve a more generous pedestrian-friendly environment;

- improved and shortened street crossings;
- 6.0 metre wide uncluttered sidewalks;
- high quality paving materials;
- new street trees, new street furniture, benches, lighting, transit shelters; and,
- other features which will provide a range of amenities.

The Streetscape Concept, Principles and Elements for Jasper Avenue are described below and will guide the preparation of detailed construction and tender documents for the renewal of Jasper Avenue.

In addition to the streetscape reconstruction, a range of policy and urban design recommendations highlighted in later sections of this report will ensure that all new development and the retrofit of existing buildings will complement and support the streetscape investment. Through high quality urban design, Jasper Avenue will evolve as a more active and vibrant place.

Figure 19  
Illustrative rendering of the New Vision for Jasper Avenue



*“There is magic to great streets. We are attracted to the best of them not because we have to go there but because we want to be there... They are symbols of a community and of its history; they represent a public memory.”*

*– Allan B. Jacobs, Great Streets, 1993*

*“Streets are more than public utilities, more than the equivalent of water lines and sewers and eclectic cables and more than linear physical spaces that permit people and goods to get from here to there.”* (Allan B. Jacobs, *Great Streets*, 1993) Streets play multiple roles. They provide a physical context for urban living, and often represent the places in which economic and cultural exchange take place. In higher density urban environments, streets constitute the public living room for many people when they are not indoors.

In the last half of the 20<sup>th</sup> Century, some roles that streets play have become over emphasized. In North America, the movement and transportation function of a street is more often considered than its social and economic exchange functions, or its contribution to quality of a place. Streets have a civic and a political role, providing a location for the celebration of important community events or public demonstrations. Streets can also be places that showcase or exhibit what a society has to offer, conveying a sense of underlying community values and aspirations.

Most cities have one or two preeminent streets,

which symbolize the defining characteristics of a city. Michigan Avenue in Chicago, Regent Street in London, and the Champs-Élysées in Paris represent well known international streets which aptly convey the best and defining characteristics of a people and place. Canadian cities also have great streets. Montreal is well known for streets like St. Catherine or St. Laurent Streets; Toronto for Yonge, Bloor or Queen Streets; Calgary for Stephen Avenue or 17<sup>th</sup>; and Vancouver for Robson or Granville Streets. Edmonton has Jasper Avenue north of the river and Whyte Avenue, south of the river, which are quite different from one another but are undeniably Edmonton’s preeminent streets.

Over time, cities work hard and take deliberate action to ensure that their signature streets continue to live up to their name and appropriately represent the image they want to portray. Across North America, many cities have recently completed or are engaged in significant improvements to their signature streets to refresh them, attract continued investment and better reflect the vitality, aspirations and achievements of the city.

Jasper Avenue is the City’s preeminent main street and as such, reflects the City’s values, aspirations and achievements. The New Vision for Jasper Avenue will reconfigure the right-of-way to provide a better balance for all modes of transportation and more space dedicated to pedestrians and to transit users. Jasper Avenue will be better able to serve the entire city and link many core assets within the downtown. The Streetscape Concept proposed will enable the street to more successfully fulfill its multiple roles – a place for social, economic, cultural and civic exchange, as well as a movement corridor for pedestrians, cyclists, transit users and vehicles.

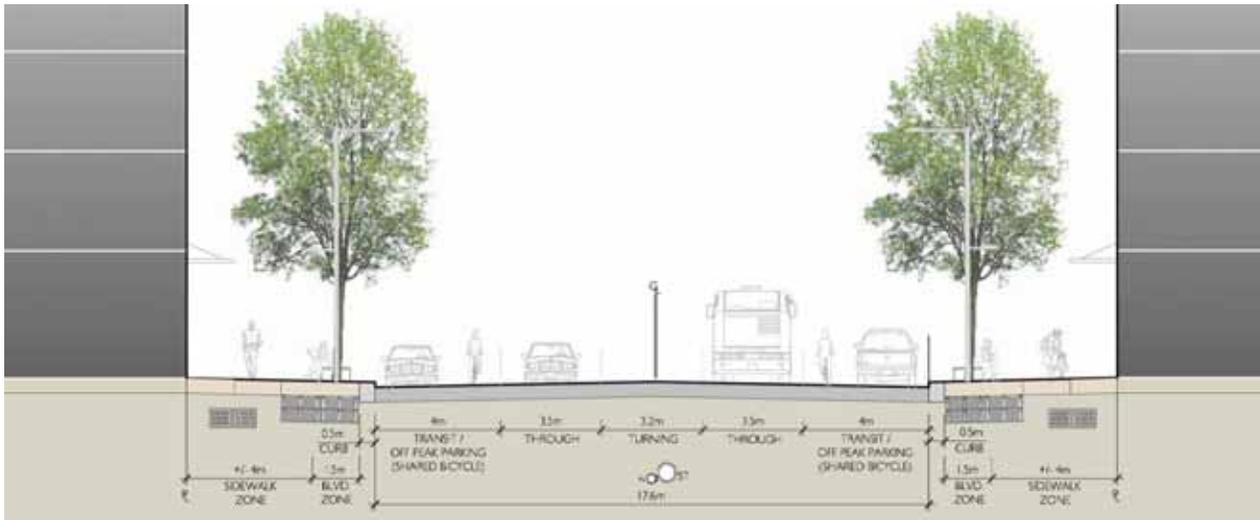
## Reconfiguration of the Jasper Avenue Right of Way

A key component of a liveable downtown and vibrant streets is a mobility system that effectively serves all modes of transportation – walking, cycling, transit and automobiles. To this end, reconfiguration of the Jasper Avenue will lay the foundation for future growth on the corridor and prepare it to re-emerge as a commercial shopping destination. Widening the sidewalks will better align the mobility function of Jasper Avenue with the goals of the City's Transportation Master Plan, prioritizing pedestrian and transit movement.

Jasper Avenue shall be reconfigured, generally in accordance with the following recommendations:

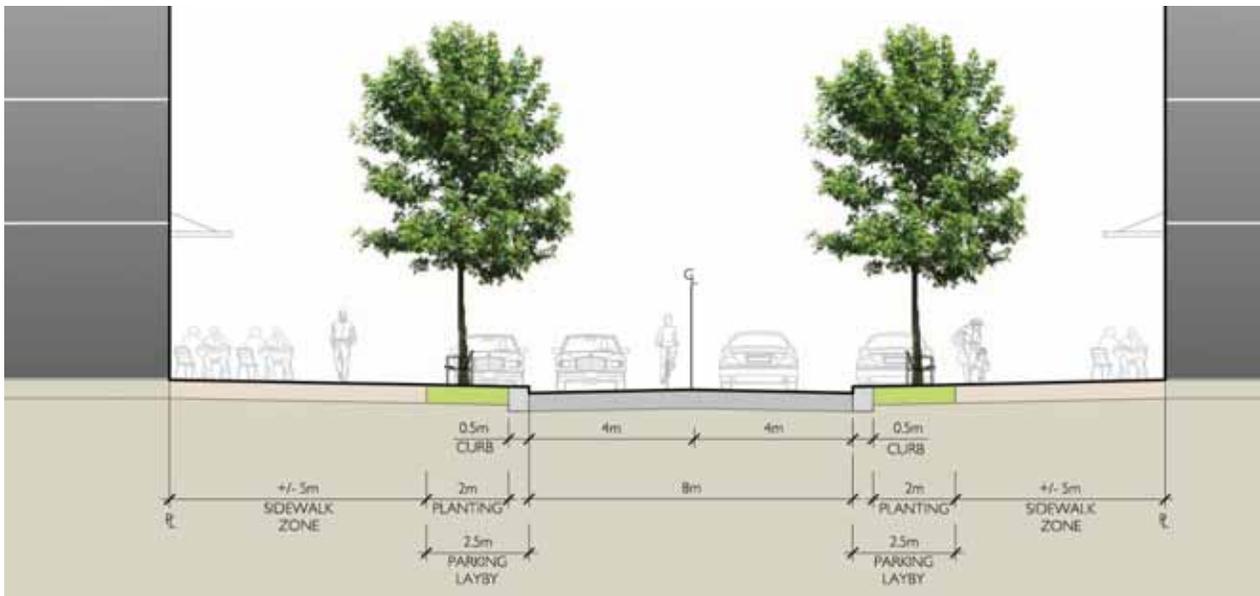
- 4-lanes of through traffic and a centre turn lane;
- A total curb to curb width of 18.2 metre;
- Approximately a 6-metre wide pedestrian realm on either side of the street;
- A straight curb line across the corridor (except at two bus bays);
- Bus bays at 101<sup>st</sup> Street to be retained at 65 metre length;
- All existing bus stops to remain;
- 4-metre curb lane to facilitate a shared automobile and bike lane;
- Two scramble (all directions crossing phase) intersections: at 104<sup>th</sup> Street and 108th Streets; and,
- New paving, lighting, way-finding infrastructure, street furniture and signage.

Figure 18 illustrates the proposed Jasper Avenue's right-of-way section. Not all blocks, however, will be reconfigured in a uniform manner. Various blocks along the street will serve different functions depending on whether they facilitate transit stops or station entrances and other unique circumstances. The Streetscape Zones, highlighted in Figure 19, shows three different types of zones: Optional Gateway Zone ; Transit Zone; and New 5 Lane Zone.



▲ Figure 20  
Typical New Five-Lane Section with Generous Pedestrian Realm

A capacity review of Jasper Avenue at a macroscopic level indicates that the removal of a traffic lane in each direction, to 5-lanes, would not degrade operations to unacceptable levels. Edmonton's connected network of downtown streets provides sufficient capacity and a range of route choices and alternatives to Jasper Avenue. These changes reflect the policy direction and framework of the Transportation Master Plan and the CCDPlan, as well as recent strategies completed by Transportation and Community Services including the Sidewalk Strategy, Bicycle Transportation Plan, and Walkability Strategy.



▲ Figure 21  
Proposed Section for Side Streets

The New Vision proposes that selected side streets on either side of Jasper Avenue be reconfigured to accommodate some relocated utility functions, as well as to narrow crossing distances.

## Jasper Avenue Streetscape Principles



### A palette of high quality and consistent design and streetscape materials

Quality, consistency and a logical degree of repetition of streetscape materials such as paving, lighting and street trees convey that an area has been planned and ordered to best suit user needs and is a place for people. These characteristics provide users with a measure of certainty and security. Consistency must be balanced with opportunities for active engagement and diverse ways of occupying the street.



### A memorable place of surprise, delight and expression occurring at differing scales

Great Streets are more than the sum of their parts; they are responsive to the multiple needs of their users, accommodating a diverse range of scales of activity and engagement. They provide opportunities for private quiet reflection at an intimate scale, and collective engagement at a grand scale- and everything in between.



### Transparency

A transparent street is easy to navigate because it is perceived to be open and uncluttered, with visual permeability of its edges. Great streets are conducive to movement from side to side, end to end, from the outside in and the inside out. A high degree of transparency between the indoors and outdoors provides a more interesting pedestrian experience that continuously changes as people move along the street. Transparency and architectural interest creates a dynamic background for gathering, socializing and shopping.



### Generous uncluttered pedestrian areas

People will linger where they feel comfortable, welcome and safe. Wide sidewalks with uncluttered walking areas, a high degree of visibility down the street, choices for outdoor dining areas, high quality landscaping, way-finding signage and pedestrian amenities such as comfortable benches, shelters and lighting are all important characteristics which make people feel comfortable, welcome and safe.



### Authenticity

Authenticity refers to a truthfulness of origins, attributes, commitments, and intentions. Authentic streets express regional context through their materials, colour, pattern and seasonality, and graciously accommodate the social and cultural life of the immediate neighbourhood.



### Placemaking

Streets that express 'place' have a distinct character, a strong sense of community and a high quality of life.



### Connections

A great street is a coherent part of a larger network of civic streets, plazas and open spaces. Its connectivity to the greater traffic and pedestrian realm is both legible and enticing. Key points of connection to adjacent neighbourhoods and other amenities are celebrated as gateways or thresholds to another kind of civic experience.



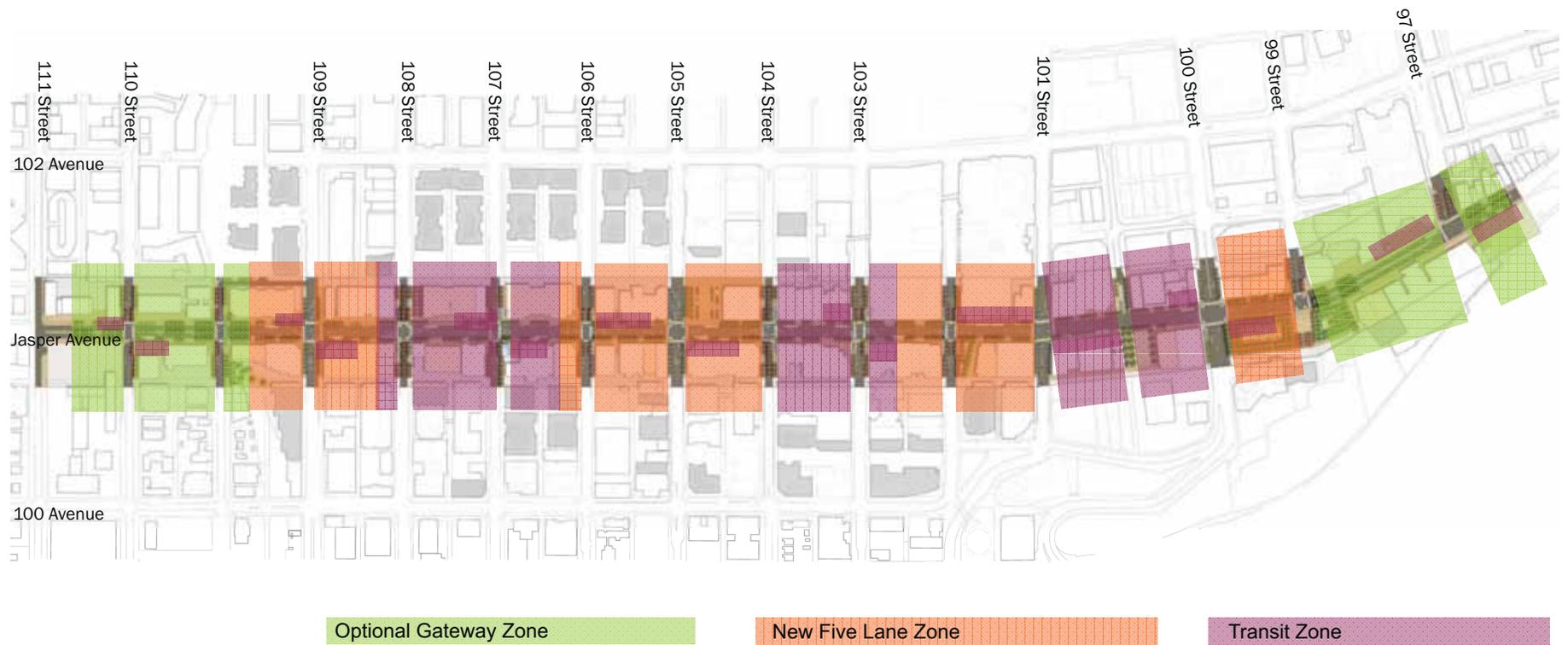
### Comfort

Great streets are designed with a strong consideration of microclimate in terms of sun, wind, and shade, as well as the unique needs of the elderly and people with disabilities in getting to, moving through and connecting with their surroundings.

## Streetscape Zones

Three primary streetscape zones have been identified along the length of Jasper Avenue between 111<sup>th</sup> Street and 97<sup>th</sup> Street: the new 5-Lane Zone, the Optional Gateway Zone, and the Transit Zone.

The following depict typical sectional conditions illustrating the key physical parameters of each zone, as well as the typical configuration for side streets perpendicular to Jasper Avenue.

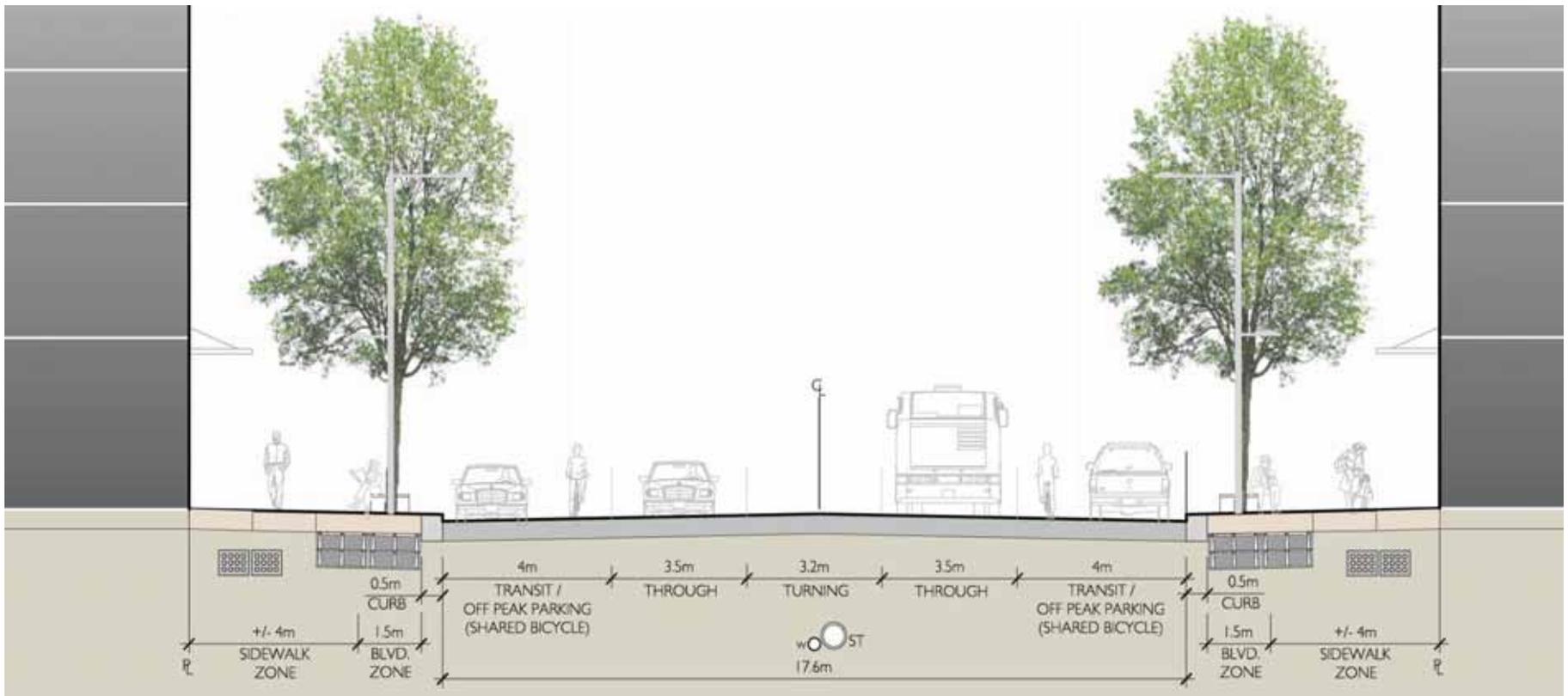


▲ Figure 22  
Streetscape Zones

## New Five Lane Zone Section

The new Five-Lane Zone section, like the Transit Zone section, replaces the existing central planted boulevard for a shared 3.2 metre turning lane. It also has a 3.5 metre through road in each direction and a wider 4.0 metre curb lane for transit, shared cycle use and off peak parking. The public realm is a minimum 6 metre width, which includes a 4.5 metre sidewalk area and

a 1.5 metre planted boulevard and furniture area. The sidewalk area includes 1.5 metre for cafe and shop spill out. The sidewalk is lined with a robust monolithic curb comprised of 500 millimetre steel reinforced curb.



▲ Figure 23  
New Five Lane Zone Street Section

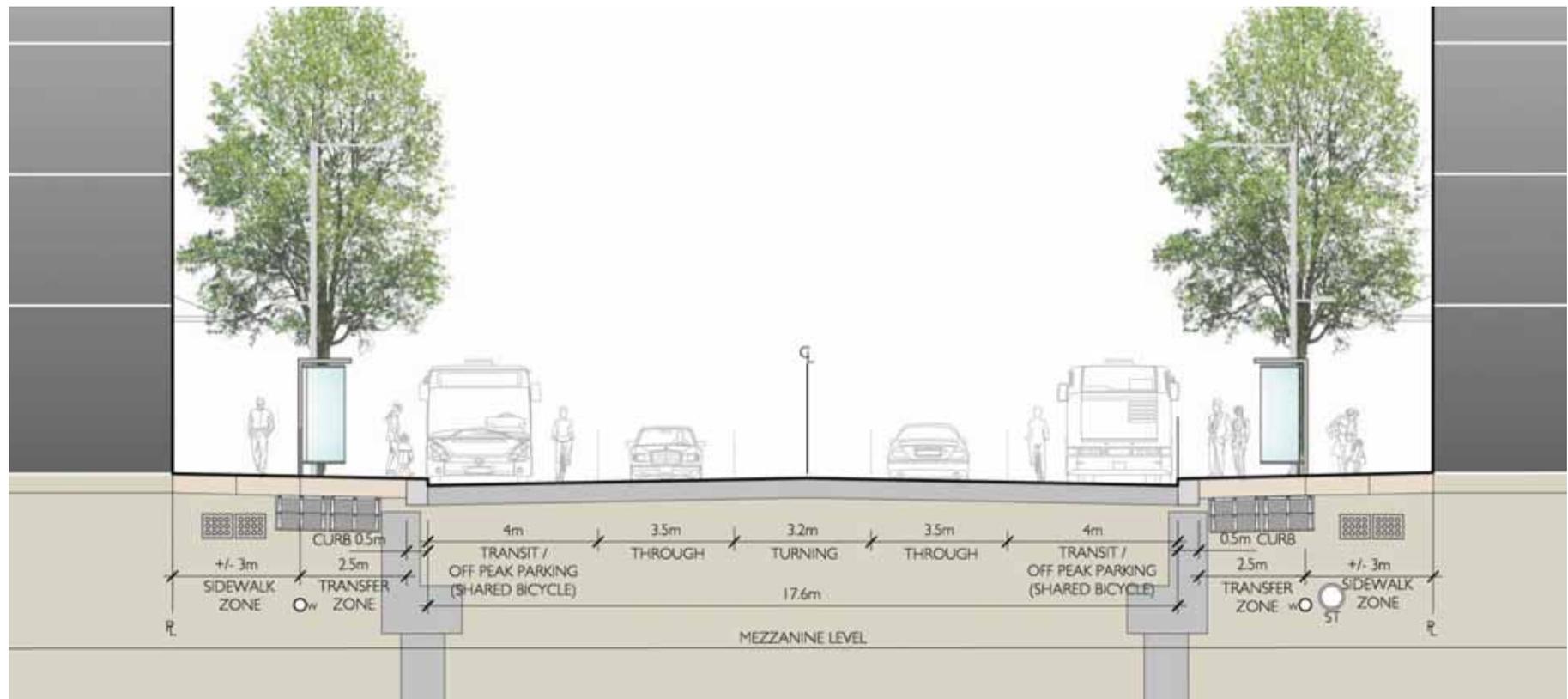
## Transit Zone Section

The Transit Zone section is the area of the street that has LRT stations located below-grade. Like the New Five Lane Zone section, the Transit Zone has a shared 3.2 metre turning lane, and a 3.5 metre through road with a 4.0 metre dedicated transit lane corridor shared with bicycles and off peak on street parking. As in all zones, the public realm is also a minimum 6 metre

width. The Transit Zone includes a 3 metre sidewalk zone, and a 2.5 metre transfer zone (for universal clearance) with street trees, lighting and furniture. The sidewalk zone includes 1.5 metre for cafe and shop spill out. The sidewalk is lined with a robust monolithic curb comprised of 500 millimetre steel reinforced curb. The solid pile walls that support the underlying

LRT stations present a limiting factor for the location of street trees, as does the need to accommodate bus shelters along the street.

Figure 24  
Transit Zone Street Section



## Optional Gateway Zone

The Optional Gateway Zone section may be used in place of the New Five-Lane Zone section at both the Eastern and Western entry to the streetscape study area to announce the arrival to Jasper Avenue and downtown Edmonton. A central boulevard reinforces the Avenue's presence along the North Saskatchewan River Valley, and pulls the riparian landscape up into the downtown core. The Optional Gateway Zone treatments could eventually extend to 124th Street. The shared turning lane becomes a 3.2 metre planted median with stately street trees and formal plantings. As in all zones, there is a 3.5 metre through road with a 4 metre dedicated transit corridor shared with a bicycle lane. There is no on-street parking in the Gateway Zone. The public realm is a minimum 6 metre width, which includes a 4.5 metre sidewalk zone and a 1.5 metre planted boulevard and furniture zone. The sidewalk zone includes 1.5 metre for cafe and shop spill out. As in all zones, the sidewalk is lined with a robust monolithic curb that can optionally be a robust 500 millimetre steel reinforced curb. The use of the Optional Gateway Zone section will be determined at the time of detailed streetscape design.

## Typical Side Street Section

Transitions to sidestreets are announced with 2.5 metre curb bumpouts that slow traffic, present a zone for substantial tree plantings, and extend the public realm at key corner locations. Beyond these bumpouts is a 2.5 metre parking layby edged with a 300 mm curb. Sidestreets typically have 4 metre shared driving and bicycle lanes.

Figure 25  
Optional Gateway Zone Street Section

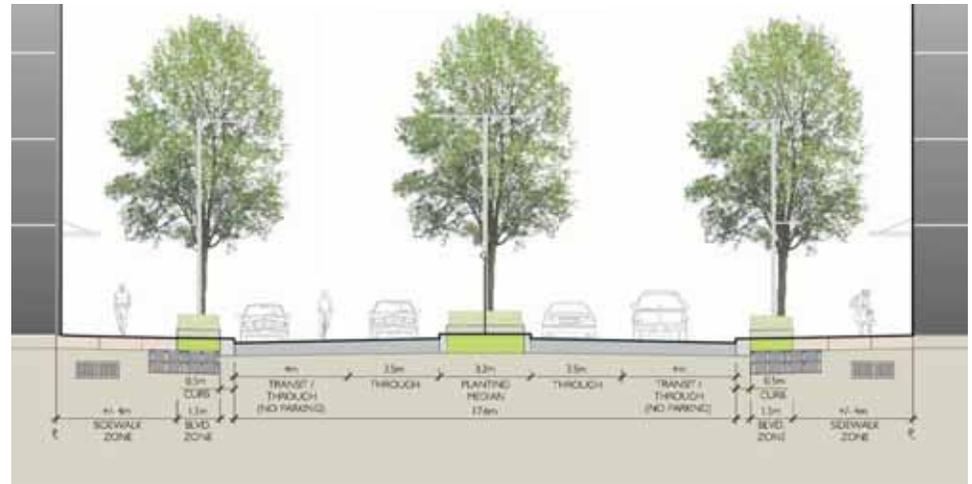
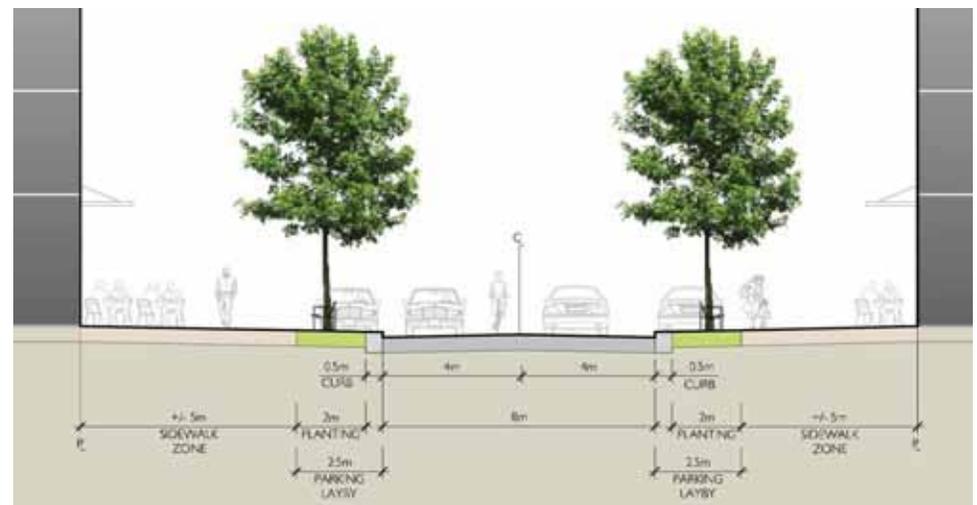


Figure 26  
Typical Side Street Section



## The Jasper Avenue Streetscape Concept

A key aspect of the Jasper Avenue streetscape is to redesign the street to exhibit a 60-40% split between space dedicated to vehicles and space for pedestrians - a significant improvement over the current automobile oriented 75-25% split. The pedestrian realm will be enhanced as follows:

- Cohesive treatment of the public realm;
- Consistent paving;
- Wide curbs and a widened walkway;
- Clear travel paths and aligned crossings;
- Café/storefront activity zones;
- Substantial tree plantings and retention of existing trees where possible;
- Unified lighting design;
- Coordinated street furnishing package;
- Transparency along Jasper Avenue;
- Universal / accessible design; and,
- Embedded winter sensibility.

An enlarged block-by-block streetscape concept for Jasper Avenue is presented in Appendix B of this report.



## Side Street Streetscape Concept

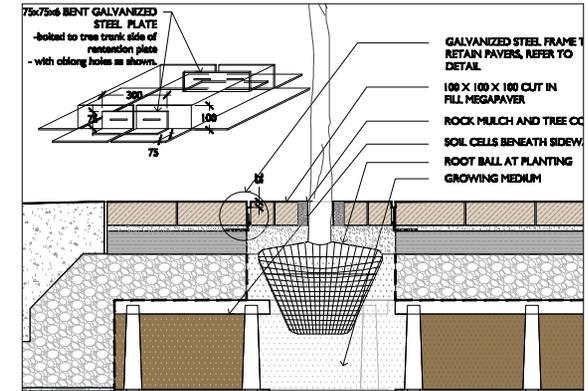
The following are considerations for selected side streets within the study area:

- Relocate utility kiosks and postering;
- Placement of transparent LRT entries;
- Thresholds to surrounding districts; and,
- Incorporation of secondary site furnishings.

Figure 27  
Jasper Avenue Streetscape Concept Plan



## Streetscape Elements



### 1. Street Trees

The new Jasper Avenue Streetscape creates a more equitable balance within the overall public ROW between the street and the sidewalk. The new design will feature a contemporary design with wide uncluttered sidewalks, new street furniture, and a healthy tree-canopy.

A number of elements have been identified as the building blocks of the street, and have been selected and evaluated in terms of how they can best accentuate the overall public realm. These elements should all be seen as a permanent investment and should be supported by a rigorous maintenance program. These include street trees, paving treatment, and curbs and crossings. To date, the design development phase has explored durable sidewalk materials and best practices in construction methods, including metal faced curbs.

Performance guidelines, which will be produced as part of the functional streetscape plan, a deliverable of this initiative, will contain design filters and details to be developed for these three elements considering: sustainability and energy efficiency, climate correctness, Jasper Avenue's character, identity, accessibility, longevity, cost effectiveness, and maintenance.

A tree planting strategy is a key element of civic infrastructure that enhances the attractiveness, comfort and safety of the street. Not only do trees elevate the civic status of the street, they help to mitigate urban heat island effects, absorb and filter stormwater and provide habitat. They also slow the pace and intensity of street activity and reduce pedestrians' perception of traffic volume and speed, ultimately creating more desirable places in which to linger, socialize, and shop. Close attention should be paid to the conditions in which they are planted, the tree species selected and to their long term maintenance.

Robust street tree plantings will establish a new and consistent identity along the length of Jasper Avenue and will serve to reconnect Edmonton's downtown core to the riparian landscape of the North Saskatchewan River which intersects with Jasper Avenue to both the east and west. Important considerations in the development of the street tree strategy are:

- An existing street tree assessment and a protection, relocation and replacement strategy should be developed in order to capitalize on Jasper Avenue's existing street trees, prior to undertaking detailed design drawings.
- Trees should be recognized as an essential civic utility, thus securing the subsurface soil volumes that are necessary for the growth of substantial, healthy tree canopies. Structural soil cells should be utilized to maximize root access to required soil volumes.
- Large canopy broadleaf trees should be selected for disease resistance and distinct winter form.
- Planting details that recognize the substantial stormwater management role that street trees can play should be developed.
- Electrical connections should be incorporated for seasonal lighting displays.



## 2. Pavement Treatment

Paving is the most pervasive streetscape element, and thus contributes most to the lasting impression of the street. Varying paving materials, paver dimensions, colours, and textures communicate distinct streetscape zones and transition areas, as well as contribute to the visual coherence of the street. Light coloured paving materials can serve to mitigate the urban heat island effect by reflecting rather than absorbing the summer sun. Durability to snow clearing equipment, freeze thaw cycles and general wear and tear should be of high priority.

Important considerations in the development of paving details are:

- Large module (300-400 x 600 x 100 thick) unit megapavers are recommended for legibility of the streetscape and for durability.
- A permeable asphalt base course should be used for stability and drainage.
- Paving patterns to be developed will reinforce street zones and streetscape hierarchy.
- Custom colour and finish to be determined.



## 3. Curbs and Crossings

Curbs and crossings are the interface between the pedestrian and vehicular realms and need to be robust and clearly demarcated to enhance pedestrian safety. Each is an extension of the paving pattern that demarcates movement and the limits to movement, and must be understood in terms of their contribution to the overall streetscape coherence and expression. Important considerations in the development of the curb and crossing details are:

- Wide (500 mm) monolithic cast-in-place concrete curbs are recommended for durability.
- Metal armoured curb faces are recommended options for snow plow protection and longevity.
- Pedestrian scramble crossings are recommended for selected intersections
- Timed pedestrian signals should be used for priority movement.
- Drainage and stormwater management should be integrated.



- ▲ This recommended sidewalk construction detail combines the recommended details developed for each of the key elements: street trees, paving, and curbs.



- ▲ Integrated storm drains

## Supporting Streetscape Elements



A variety of supporting elements will serve to ‘furnish’ the street, enhancing its value as a ‘civic living room’ for the city and include:

1. Site furnishings and seating
2. Street and pedestrian lighting
3. Signage and traffic signals
4. Side street planting and stormwater management features
5. Transit shelters and station entrances
6. Public art
7. Seasonal and feature lighting
8. Special paving, facades and storefront signage
9. Advertising and information



The development and selection of these elements will be determined during the detailed design process. Each element will be evaluated with performance design guidelines considering - sustainability and energy efficiency, climate correctness, Jasper Avenue’s intrinsic character, identity, accessibility, longevity, cost effectiveness, and maintenance- but also perspectives unique to each element.

For example, site furnishings and seating, and advertising and information performance guidelines will consider the following:



### Site furnishings and seating considerations

- Size and scale: should be considered relative to people, streets and buildings.
- Coordinated family of elements: should collectively express a cohesive and organized design language and program.
- Universal design: should be inclusive of, and be easy to use by all user groups.
- Safety and security: should be made from safe materials, and correctly placed for night visibility and safety concerns
- Materials and fabrication: should be made from durable, vandal resistant materials, and be easily maintained and repaired.



- Flexibility: should be easy to install and flexible to rearrangement.
- Sustainability: should be considered for material content and energy use.
- Identity: should be consistent with the civic image of Edmonton, as well as the various precincts of Jasper Avenue.
- Placement guidelines: placement should be considered for order, spacing, clearances, and sightlines.



#### Advertising and information considerations

- Integration and maintenance contracting
- Quantity: extent of advertising, placement and visibility of advertising.
- Information kiosks, ticket dispensers, signage
- Ongoing maintenance, repair and replacement of advertising
- Outdoor media and industrial design companies: Astral Media, CBS Outdoor, ClearChannel Outdoor, Enseicom, KDA





▲ Figure 28  
Existing and Proposed Conditions for Jasper Avenue

## Functional Streetscape Plan Development

The Jasper Avenue Streetscape Concept will provide the basis for the development of a functional streetscape design as part of the new vision initiatives anticipated to be completed by the end of 2010. Key steps in the creation of the functional streetscape design include:

### Coordination of Infrastructure and Utilities

The streetscape design will require a comprehensive plan that coordinates the spatial design and functionality of the street with the precise requirements of all civic infrastructure and utilities.

### Coordination of Transit Operations and LRT Station Repairs

Similarly, due to the proximity of streetscape construction, LRT Station repairs and ongoing transit operations, a comprehensive coordination plan will be required.



### **Construction and Maintenance**

Ongoing construction and maintenance of all elements of the streetscape will be planned and synchronized.

### **Phasing and Implementation**

An implementation and phasing strategy will be developed that considers all stages of the project and their effects on adjacent infrastructures.

### **Special Challenges**

Special challenges will be identified and considered in the overall development of the final streetscape proposal.

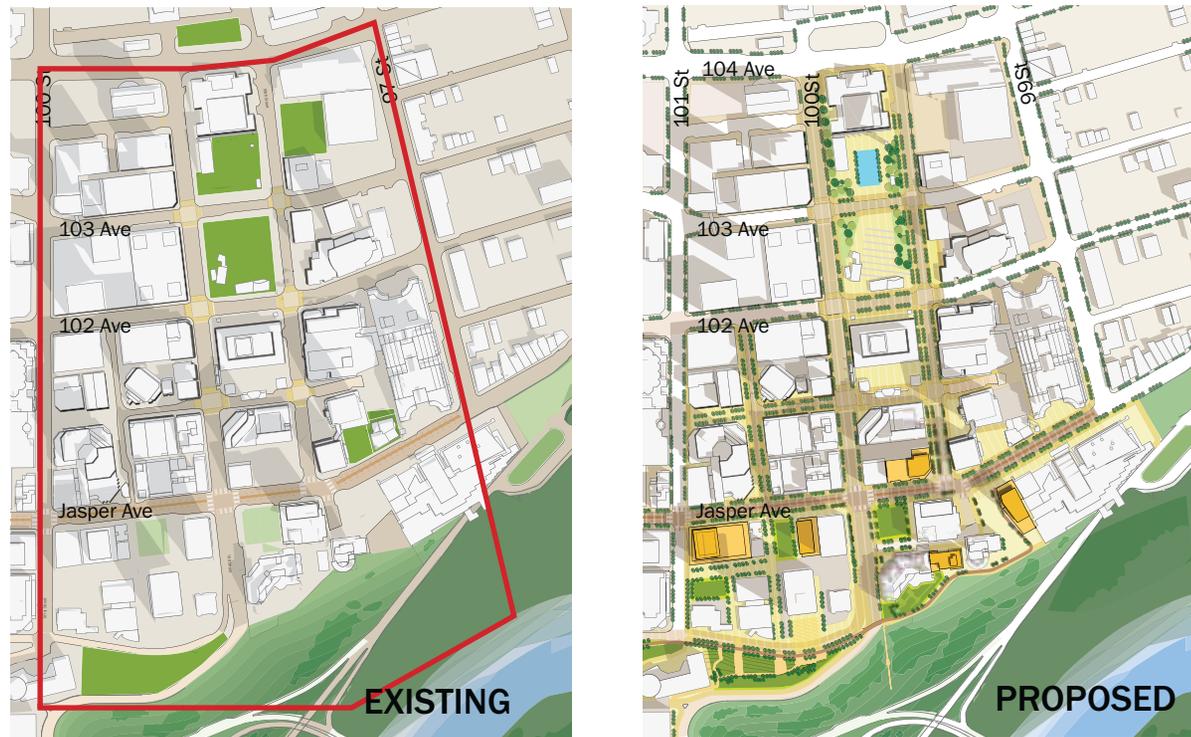
### **Order of Magnitude Costing**

Order of magnitude costing will be prepared for the direct construction costs of the streetscape. Direct cost must be augmented with an estimation of design fees. Specialized services such as detailed structural engineering costs, other contingency and escalation cost achieve the total project cost.

## Big Move #2: Create a permanent public park in front of Hotel Macdonald and reinforce connections between Jasper Avenue, the riverfront and the Civic District through a range of civic precinct public realm recommendations

A range of public realm improvement projects have been identified to enhance the Civic Precinct and strengthen its emergence as an area for business, cultural and civic life. Below is a proposed list of public realm improvement projects.

Figure 29  
Existing and proposed public realm condition along Jasper Avenue



Bryant Park, New York City

### Hotel MacDonald Parkland Acquisition

The existing open space in front of the Hotel MacDonald is not in public ownership, however it is one of very few open spaces that front directly onto Jasper Avenue. This open space is an important asset within the downtown, as it functions as a crossroads marking the visual and physical access to the river valley to the south, City Hall and the range of cultural amenities to the north, as well as frames one of the City's best loved and most prominent landmark buildings, the Hotel MacDonald. As such, it is recommended that the City take steps to permanently acquire this space and protect it as a public park. There are a number of mechanisms the City could explore to acquire this space beyond a straightforward land purchase, including: the potential for a land swap or an exchange of existing municipal surplus lands (potentially across multiple sites); provision for the transfer of existing density permissions from this site to other land holdings in the current land owners portfolio to reduce the land value and subsequent cost of land purchase; and/or the establishment of a trust to solicit corporate and individual donations to assist in securing this important site. The municipality may pursue a number of these mechanisms in concert. Once in public ownership, this new park space could also be the subject of an international design competition to create a signature public gathering space in the heart of downtown.



### 99<sup>th</sup> and 100<sup>th</sup> Streets Streetscape Improvement Study

99th and 100th streets frame the Civic Precinct and fall on either side of City Hall and Churchill Square extending south and intersecting Jasper Avenue. South of Jasper Avenue, 100th Street also provides an important connection to the River Valley. This part of downtown is a focal point for civic and commercial life and includes a significant concentration of jobs and corporate office locations. The area surrounding Churchill Square is home to a number of citywide festival venues, civic buildings, hotels and world class cultural destinations, and collectively, these all play a significant role in Edmonton's tourism and cultural economy.

A streetscape improvement study is recommended for both 99th and 100th Streets between 104th Avenue and Jasper Avenue to strengthen connections to Jasper Avenue, and enhance the overall character of the Civic Precinct. The study should consider an upgrade of pedestrian paving materials, locations for public art, the potential for expanded sidewalks and roadway narrowing, integration of new street furniture, enhanced tree-planting and landscaping. The study could also explore the extent to which Jasper Avenue Streetscape and design elements can be extended into the Civic Precinct.



Attractive and safe pedestrian linkage

### 100a Street and 101a Avenue (Rice Howard Way) Streetscape Improvement Study

Similar to 99th and 100th Streets, 100a Street and 101a Avenue are important Civic Precinct corridors, which have received streetscape enhancement in the past. Today, these streetscapes, particularly the paving materials, streetscape furnishings and landscape character appear dated and could be renewed to support the emergence of a unique restaurant and outdoor dining/entertainment district attracting many more of these types of uses to the area.

A streetscape study is recommended, which aims at creating a renewed pedestrian-oriented environment capable of supporting high quality secondary retail and outdoor dining. As these are both short, discontinuous streets, the study should examine the potential for roadway narrowing, expansion of sidewalk areas and potential long term closure of the street to traffic during daytime hours or special events. The study should also examine ways to improve the urban character of the District and its landscape design qualities, including paving materials, tree planting and street furniture. Long term strategies to mitigate the negative physical presence of the parking garage entry at the corner of 100a Street and 101a Avenue should also be explored.

### College Walk Public Realm Improvement Study

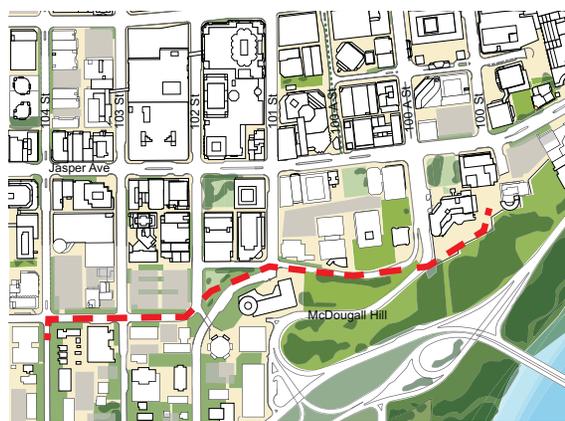
The block bounded by 100th Street to the east, Jasper Avenue to the north, 101st Street to the west and MacDonald Drive to the south is a large block, which presents a number of interesting public realm opportunities. In consultation with private land owners, the City should undertake a public realm improvement study to seek ways to better link Jasper Avenue to the River Valley edge and the Heritage Trail. This linkage could effectively extend a rejuvenated Rice Howard Way south across Jasper Avenue to reinforce connections to the river valley and to establish a more attractive environment for the range of uses and potential new uses on this block. This block also has potential to yield small gathering places and parks which could significantly add to the amenity and draw of the Civic Precinct.

### Big Move #3: Complete the Heritage Trail along the River Valley edge and create a new pedestrian mews linking Beatrice Carmichael Park to Veterans Park

The Heritage Trail, situated along the top of the North Saskatchewan River Valley is interrupted at the east end of the study area. For many years, people have had a strong desire to see the network linked across 100th Street via a bridge and continued east across the valley edge south of the Hotel MacDonald. To the west of 100th Street, Heritage Trail could be extended across a rejuvenated Veterans Park and realigned from 100th Avenue to a series of connected landscaped mews from Veterans Park to Beatrice Carmichael Park.



▲ Figure 31  
Existing Heritage Trail



▲ Figure 30  
Heritage Trail



▲ Figure 32  
Proposed Heritage Trail



### **Riverfront Heritage Trail completion study**

The eastern end of the Heritage Trail, has a significant missing link in the vicinity of 100<sup>th</sup> Street and adjacent to the Hotel Macdonald. The completion of Heritage Trail system should be a high priority to reinforce this well utilized public realm and recreational resource in the downtown.

The City should undertake a study to facilitate the completion of the Heritage Trail to connect downtown to the emerging Quarters District to the east. This initiative will require significant architectural and engineering components for the design of a pedestrian and cycling bridge over 100<sup>th</sup> Street. It is also likely that a cantilevered walkway will be required to the south of the Hotel MacDonald on the river valley edge. These structures present unique opportunities for contemporary design expressions and amenities that will provide unparalleled views to the river valley below. In addition, the study should seek to significantly improve the character of the river valley promenade on MacDonald Drive.



### **Veteran's Park improvement and the creation of a mews linking to Beatrice Carmichael Park and 104<sup>th</sup> Street**

Along the mews, buildings and redevelopment sites should include active retail frontages, creating a new setting for unique social, cultural and commercial activity. Uses could include art galleries, restaurants, outdoor dining establishments and convenience retail to support both the working and residential populations envisioned for this area. The City should take steps to create the mews through land acquisition and/or other mechanisms provided for in the CCDPlan. The mews should feature high quality hard landscaped paving, trees, public art, street furniture and lighting.

Along the mews, a wide range of development opportunities exist including high rise mixed use, office uses as well as mid-rise developments. Streetscape improvements on 104<sup>th</sup> Street will link this unique open space armature to Jasper Avenue to the north, and a new stairway, between 99<sup>th</sup> Street and 98<sup>th</sup> street, will connect to significant residential development planned along the Lower Valley.



▲ Figure 33  
Model view of proposed mews linking to Beatrice Carmichael Park and 104<sup>th</sup> Street

## Big Move #4: Promote intensification of the Capital District as a place for new mixed use and office developments

There are a number of vacant and underutilized sites in the Capital District south of Jasper Avenue that detract from the character of this important office and government precinct. Office development will continue and be supported by additional mixed use, residential, and hotel uses.

Small-scale retail and restaurant uses will provide amenity for the local employment and residential populations and improve the vitality of this district beyond the work day.

The Capital Boulevard streetscape initiative is currently underway along 108<sup>th</sup> Street and will be an important corridor linking the Capital District to Grant MacEwan College. The interface of the Jasper Avenue and Capital Boulevard streetscapes should be carefully considered to ensure compatibility.



Figure 34  
Capital District

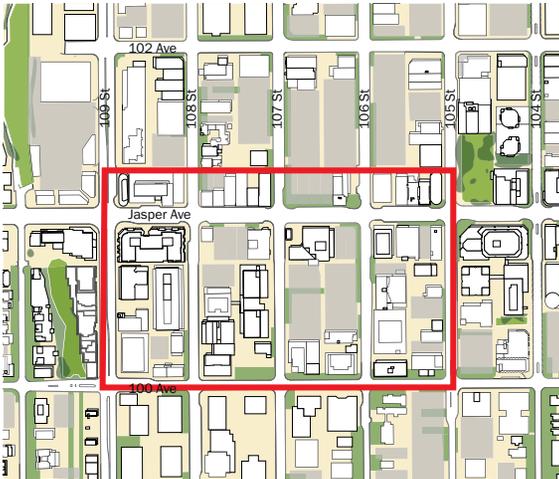


Figure 35 Existing Condition of the Capital District

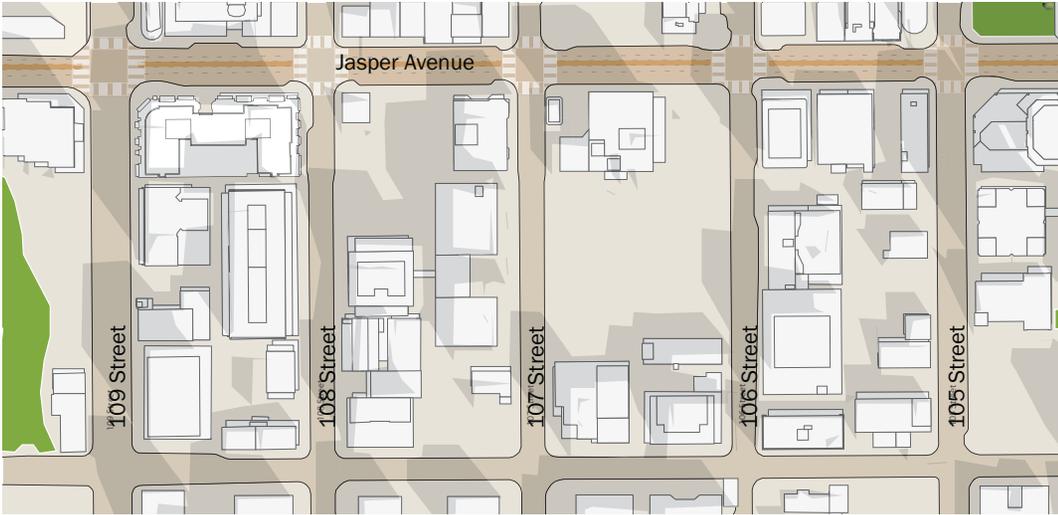
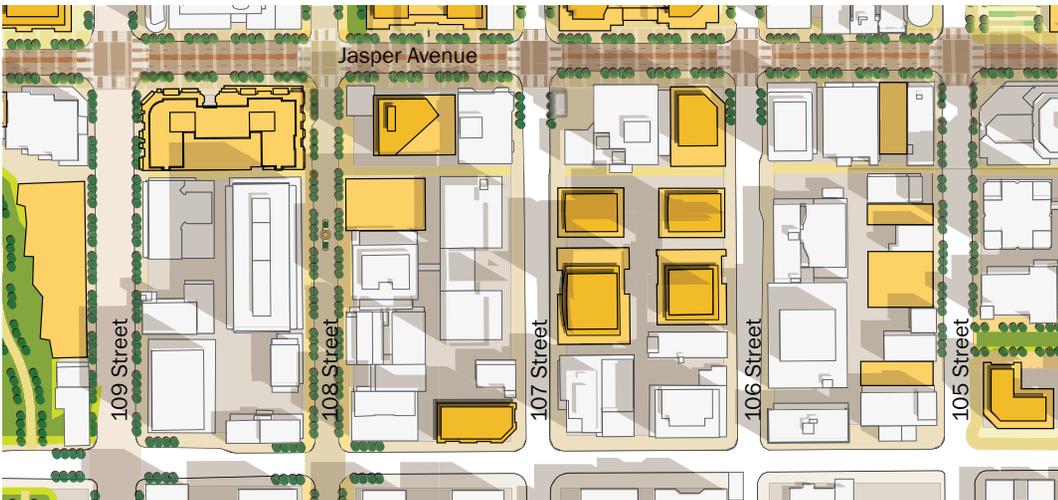
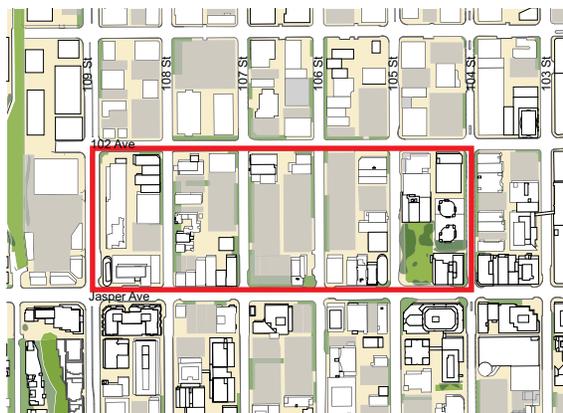


Figure 36 Proposed Condition of the Capital District

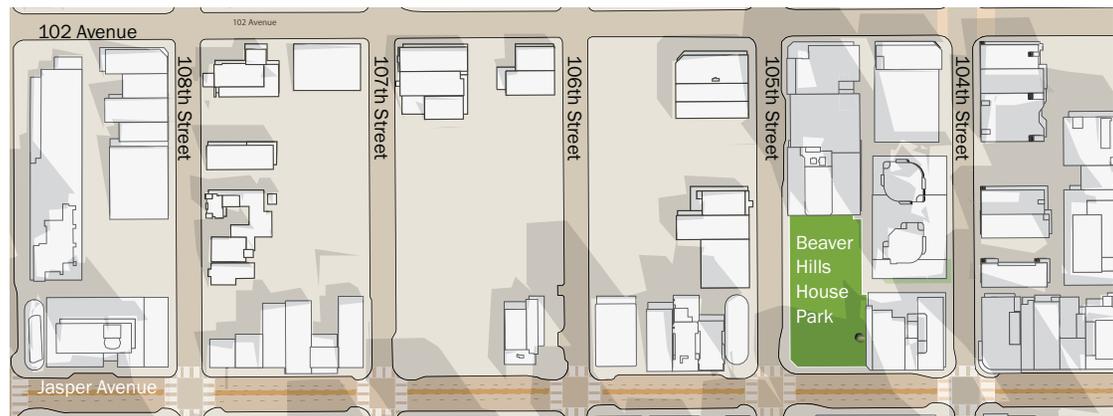


**Big Move #5: Utilize the largest underutilized and vacant redevelopment sites to make a big impact downtown: create a new high density warehouse community and new central park just north of Jasper Avenue between Beaver Hills House Park and 108<sup>th</sup> Street**

Not all redevelopment sites within the Jasper Avenue study area are alike. Many sites are small portions of blocks or infill sites and few allow for major change with a big impact. However, the area north of Jasper Avenue between 105th and 108th Streets presents a major redevelopment opportunity comprising almost three full City blocks, including street frontage on Jasper Avenue. This redevelopment area presents a significant opportunity to redefine a large district within downtown and introduce a comprehensively planned, high density urban neighbourhood with a significant new public park space.



▲ Figure 37  
Warehouse Community Park



▲ Figure 38  
Existing Condition of the Warehouse District



▲ Figure 39  
Proposed Condition of the Warehouse District



### **Warehouse Central Park Acquisition**

The large amount of vacant and underutilized land in this area presents an opportunity to develop not only a new high density residential community, but also substantial new park. This is a rare opportunity and a big move that could significantly boost residential development in downtown. The Central Park is the largest and most significant new open space proposed in the New Vision, at 2 hectares spanning 3 blocks to create a continuous open space. It will also connect to Beaver Hills House Park at 105<sup>th</sup> Street and to 104<sup>th</sup> Street via a landscaped mews connection delivered through the Icon development. This park will create significant real estate value for the surrounding development sites and an amenity for residents that is missing today. The park is envisioned to accommodate a range of soft and hard landscaping for active and passive recreation, state of the art children's play area and public art. Residential and commercial uses will frame the park to create a safer and more active environment for residents and visitors. Pedestrian crossings should be created to facilitate safe movement at 105<sup>th</sup>, 106<sup>th</sup> and 107<sup>th</sup> Streets by treating the roadway as an extension of hard surface park area to link the open space as a whole. The City will need to employ creative methods to acquire lands through development agreements, transfers of density rights and park acquisition funds.



### **Beaver Hills House Park Design Competition**

Beaver Hills House Park is currently the only public park directly fronting Jasper Avenue in downtown, and has the potential to be a cherished place at the heart of Jasper Avenue. Today, the park is underperforming. It is trying to be too many things at once, but performs none of its intended functions well. Portions of the park feel isolated and unsafe, and the rest is cluttered and without a defined identity. This park could be significantly improved as a more open, accessible, flexible and programmable space for Jasper Avenue. A simplified design would create a safer and more welcoming gathering place. It is recommended that a national or international design competition should be launched to comprehensively redesign this space. The design solution should: identify ways to actively program the park; create a contemporary design; ensure openness, accessibility and safety; and explore ways to animate the blank or inactive urban edges to the park, including the blank wall on the Ramada Hotel on the north edge of the park. Other actions could be taken to improve the park such as the acquisition or redevelopment of the low rise office structure abutting the park to the east and integrating active restaurant and retail uses at grade.



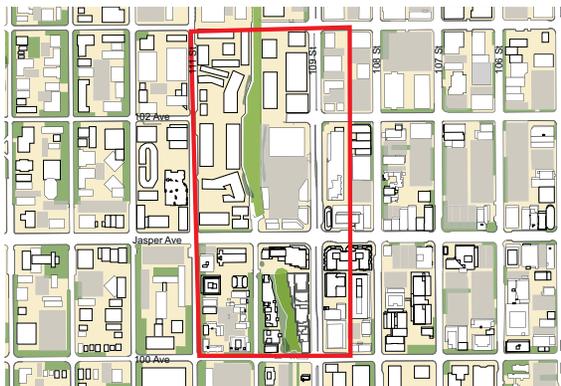
### **Continued 104<sup>th</sup> Street Improvements**

104<sup>th</sup> Street has become a significant destination in the downtown due to its remarkable heritage buildings, the summer farmers' market and the anchor Sobeys store, which has demonstrated the viability of new retail on Jasper Avenue. Significant residential intensification is planned in the vicinity of 104<sup>th</sup> Street and it is emerging as an important north-south pedestrian link within downtown. In addition, 104<sup>th</sup> will link to other open space systems including the Heritage Trail, the proposed Mews and an enhanced connection adjacent to the Icon Condominiums to Beaver Hills House Park. An additional consideration for 104<sup>th</sup> Street should include a pedestrian stair linking 98<sup>th</sup> Street to 97<sup>th</sup> Street. 104<sup>th</sup> Street should continue to be a focus for public realm improvements as highlighted in recent studies. The City should establish a resource allocation in the capital budget for proposed streetscape improvements and proceed with phased implementation.

## Big Move #6: Promote the continued intensification of the Railtown Community as a complete urban neighborhood

Over the past several years, mid-rise residential intensification and the development of the linear open space park system on the former rail corridor have created an attractive new downtown neighbourhood at the western edge of the study area. At the same time, new commercial development in the vicinity of 109<sup>th</sup> and 110<sup>th</sup> Streets has come forward in a highly suburban form, consisting of single storey retail-oriented development toward an interior surface parking lot.

Over time, this commercial area should evolve into a higher density mixed use urban neighbourhood with the commercial frontage oriented to Jasper Avenue and 109<sup>th</sup> Street. A small new park space linking to the existing Railtown open space corridor could also create an attractive entryway to the new high density community and establish a local park amenity for the District.



▲ Figure 40  
Railtown District



▲ Figure 41  
Existing Condition of the Railtown District



▲ Figure 42  
Proposed Condition of the Railtown District

### Expand Railtown Park

The Railtown Park corridor is an important and well used public park and was a key factor in attracting significant residential growth on surrounding lands. This asset can be expanded through redevelopment of the existing low-density shopping centre at Jasper Avenue and 110<sup>th</sup> Street.



Figure 43  
Foam core model of the proposed  
Railtown District



# Part Four: Land Use, Urban Design and Other Action Strategies



The New Vision for Jasper Avenue establishes an ambitious program of public realm improvements, including the Jasper Avenue streetscape, new parkland acquisition and development, enhancements to existing parkland, such as a design completion for Beaver House Hill's Park, and a range of other streetscape improvements. Collectively these public realm initiatives will establish a new public setting for the downtown which is bold and inspirational. It is also expected that these initiatives will ultimately transform the downtown into a more livable, more dynamic, walkable and cohesive network of connected places. At the same time, these improvements create significant new value for private development sites within the study area, which can unlock the potential of under-performing lands and serve to

transform vast surface parking areas into new mixed use developments and healthy thriving neighborhoods.

In this sense, investment in the public realm is aimed at securing and encouraging a corresponding investment on the part of the private sector through new high quality developments focused to the downtown. The municipality can lead this transformation of the downtown by prioritizing and phasing public realm improvements, to create a sustained program of city building activity over the next 25 years. It must be recognized this will take time, and considerable investment. To fully capture the benefits of these initiatives, it is necessary to outline a range of policy, urban design and action strategies which can articulate

aspects of the vision in greater detail and can be used by both the public and private sectors in evaluating the merits of city building propositions within the Jasper Avenue study area.

This section of the document outlines a range of policy, urban design and other recommendations in support of the vision. These recommendations are intended to support the intent of the CCDPlan approved by Council on July 7, 2010, and provide additional policy and urban design recommendations aimed at achieving a high quality of city building within the study area. Both the municipality and development community are encouraged to utilize these recommendations to guide development projects and decision making in the area. However, it is



important to note that the policies and zoning outlined in the CCDPlan shall take precedence over the Jasper Avenue New Vision recommendations. Municipal staff will review the policy and urban design recommendations outlined in this study, which may be partially or wholly integrated into a future update of the Downtown Capital District Plan and or Downtown Zoning By-law.

Policy, urban design and action strategies are presented in the following three categories:

**Land Use Recommendations** make appropriate land use recommendations within the study area in support of the New Vision. These should be reviewed by the municipality,

and, as appropriate, integrated into the CCDPlan or Zoning By-law. Action strategies in this section include a retail and commercial improvement strategy aimed at enhancing Jasper Avenue's ability to evolve as a region-wide commercial destination and desirable place to live, work and recreate.

**Urban Design Recommendations** provides specific design and performance criteria to ensure the creation of a high-quality built environment; one that enlivens the pedestrian experience and provides an attractive mixed use environment that exemplifies a commitment to design excellence and improved quality of life and place. Components of the urban design recommendations

include a built form improvement strategy; general site development guidelines and built form typologies and guidelines.

**Mobility Recommendations** outline the policy intent for the Jasper Avenue corridor to evolve as a balanced and complete street accommodating all modes of transportation, including cycling, walking and public transit.

## 4.1 Land Use Recommendations

All vibrant downtowns encourage a mix of uses and a critical mass of people to foster vibrant, active and prosperous places. Today, downtown Edmonton represents the most intense and complex land use assembly in a city comprised of high density residential developments, commercial and institutional uses, civic facilities, community amenities and open spaces. Much of the downtown exhibits these land use characteristics, however, there are areas of the downtown that are vacant or underutilized. Reurbanization of these areas is needed to create a critical mass of people and to fill gaps in the urban fabric. This will happen incrementally through public and private sector investments and actions. As the downtown grows, the City should consider undertaking a community needs assessment to determine the community and recreational facilities that are required to support and increasing downtown population. This section of the document provides land use direction and specific policy recommendations to ensure that land use will contribute positively to the revitalization of downtown. Recommendations should be reviewed by municipal staff per inclusion within the CCDPlan and/or downtown Zoning by-law.

### 4.1.1 Land Use Areas

Within the study area, it is envisioned that there will be a mix of uses including office, residential, institutional, cultural and other uses. Two broad land use designations are recommended to distinguish areas that are largely intended to evolve primarily as residential in character from those that would evolve as mixed use areas. The introduction of these two land use designations is intended to cluster and encourage mixed use development on the Jasper Avenue corridor, in the Civic Precinct and within the Capital District where mixed use and office development has tended to concentrate in the past. Residential areas are generally removed from high traffic and are highly visible areas, which are more appropriate for development as residential neighbourhoods. The introduction of a significant permanent downtown population both on and adjacent to Jasper Avenue will champion ongoing positive change, revitalize the commercial offering and significantly improve the overall safety and vitality of the area. Figure 44 below identifies land use areas in the New Vision study area.

#### Mixed Use Areas

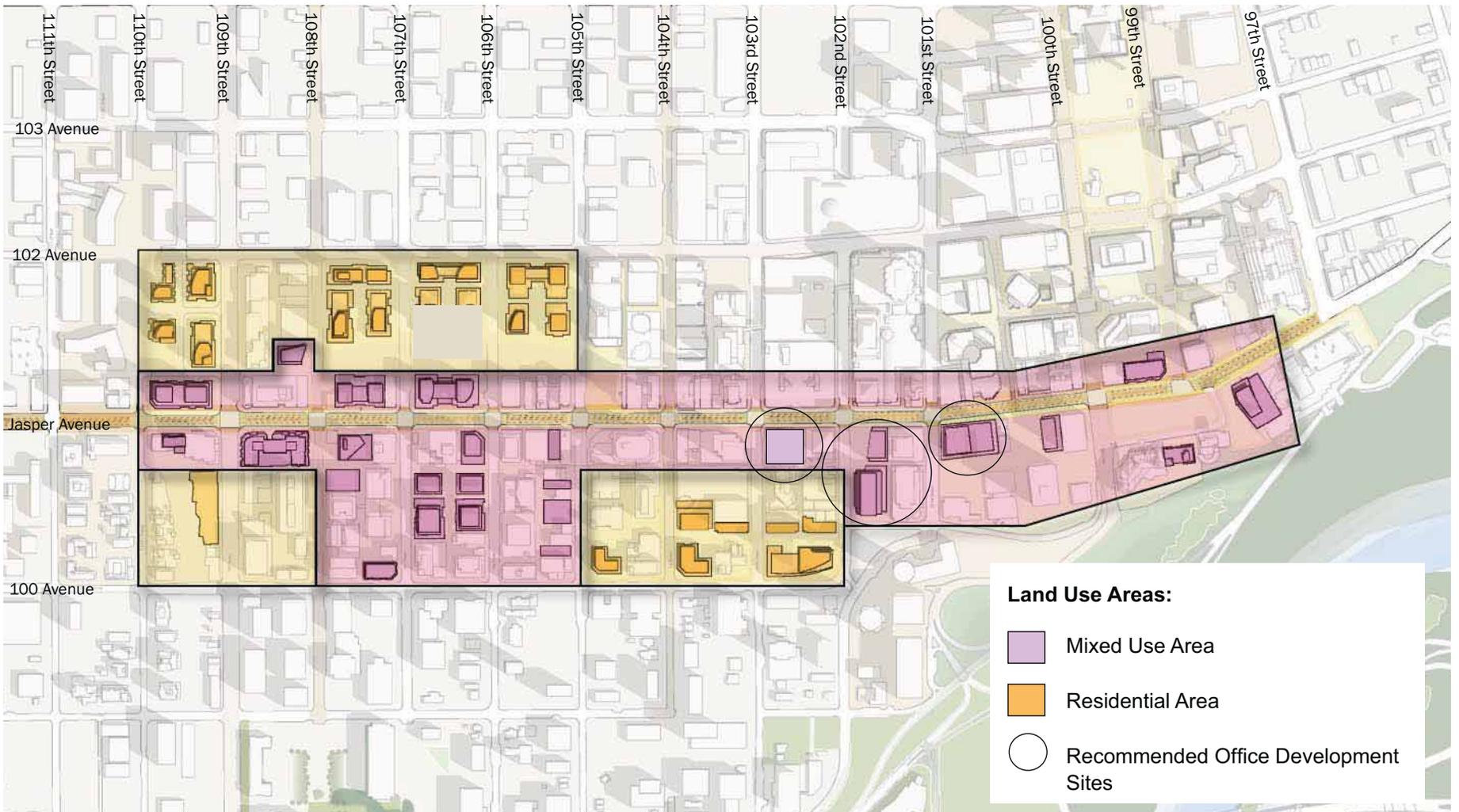
The following uses should be permitted

- a) Office, residential, commercial retail, institutional, entertainment, hotel, conference centre, business and personal services, civic, cultural, education, government and community uses, such as a library or community centre, seniors' housing and cultural uses.

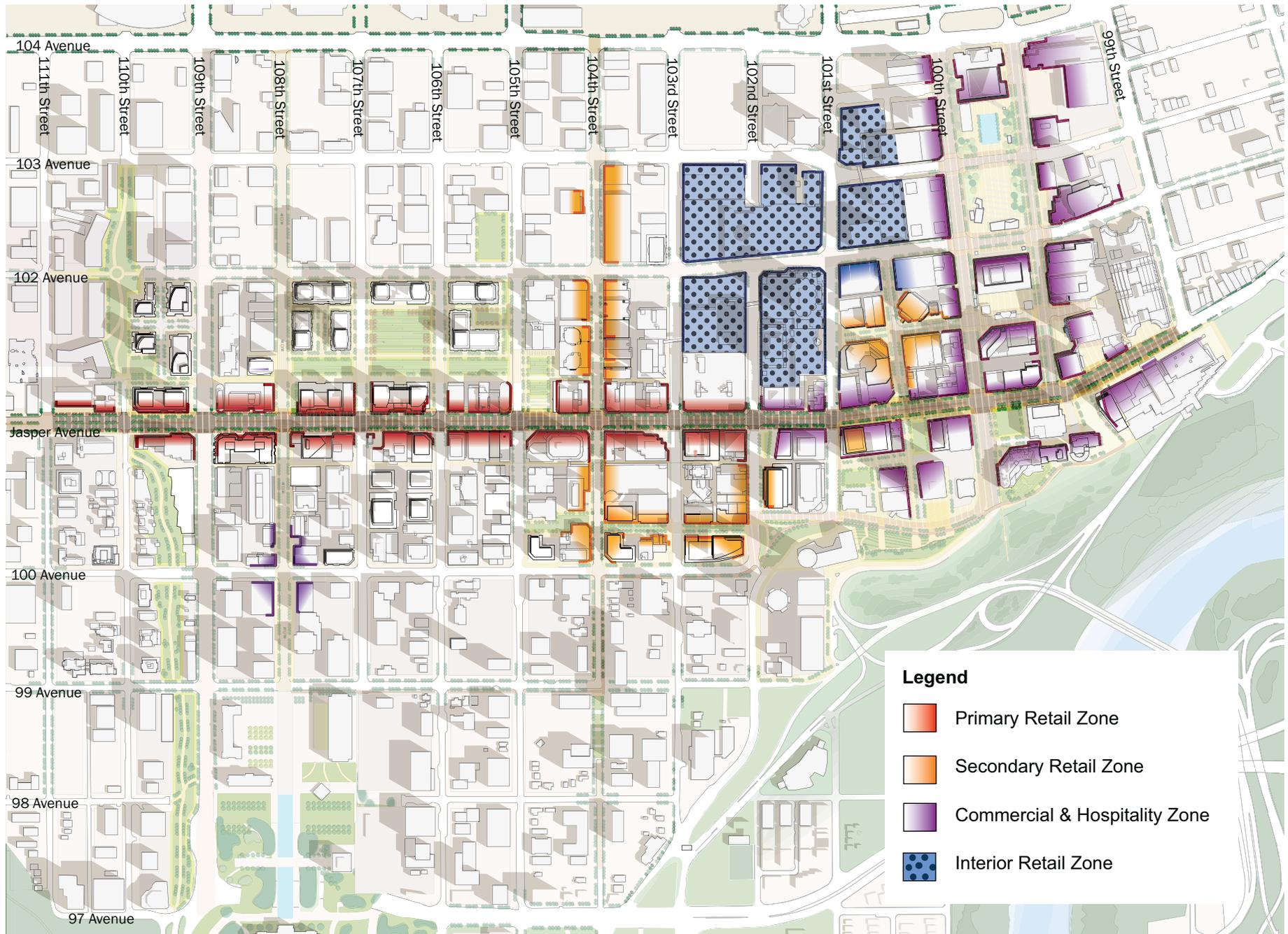
#### Residential Areas

The following uses should be permitted

- a) Mid and high-rise urban apartment dwellings, seniors housing, live/work units and community uses, such as a library or community centre.
- b) A maximum of 10% of the Gross Floor Area of a development may be used for commercial retail, and office uses.
- c) Ground floor uses in residential areas are encouraged to include grade related town house or apartment units with individual entrances fronting the public street with the exception of Jasper Avenue.
- d) Residential uses are not permitted at-grade fronting onto Jasper Avenue.



▲ Figure 44  
Proposed Land Use Areas



▲ Figure 45  
Proposed Retail Hierarchy

### 4.1.2 Retail & Ground Floor Commercial Uses

#### Prohibited Uses

Auto-oriented uses detract from the urban and pedestrian-friendly character of a downtown. The following uses should be prohibited in the Jasper Avenue Study area:

- a) Gas stations, auto parts repair and service, stand-alone retail, car washes, drive-through establishments and car dealerships.
- b) New large scale surface parking lots are not permitted; surface parking lots are not permitted fronting Jasper Avenue.
- c) Interim, small scale surface parking lots, accommodating a maximum of 30 parking spaces for primarily loading and drop-off uses are permitted, provided that these are visually screened and landscaped from the public street.



Retail and commercial uses are key anchors for main streets. Much of Edmonton's retail spending occurs in indoor retail locations or larger format suburban retail outlets. A focus on significant mixed use and residential development along Jasper Avenue will drive commercial and retail demand and will allow the corridor's retail offerings to evolve over time. A range of policy tools enforced by the City, in addition to targeted actions by the Downtown Business Improvement Association, can guide the creation of a logical retail hierarchy within downtown. Jasper Avenue should serve as a focus for dining, flagship retail, boutiques and other niche markets, as well as neighbourhood and convenience shopping.

Not all streets in Edmonton's downtown should facilitate retail use. Retail should be focused on certain streets to create a critical mass of shopping activity where appropriate. Other streets may have a secondary commercial function or have very limited neighbourhood-serving retail functions. Figure 45 establishes a retail hierarchy to focus high value active street related retail frontage where it is most desired.

Recruiting high value retail uses and creating the right merchandising mix for Jasper Avenue and other supporting retail areas in the downtown requires deliberate action. The Downtown Business Improvement Association, in concert with Edmonton Economic Development Agency, should help connect the development community with flagship, specialty

and boutique retailers who may be interested in locating in a downtown shopping environment. They can promote the New Vision and maintain a list of retail contacts. These groups can also help promote a logical merchandising mix, helping to cluster special types of commercial retail uses to various segments of the street. The City should consider undertaking a retail and merchandising mix study for Jasper Avenue.

The sections on the following pages outline the proposed retail hierarchy and corresponding policies.



## Primary Retail Zone

Primary retail includes flagship retail stores, specialty or boutique shops, restaurants and outdoor dining establishments, and other commercial uses capable of drawing a critical mass of people to a destination shopping experience throughout the day, evening and weekends. Jasper Avenue west of 102<sup>nd</sup> Street to 111<sup>th</sup> Street is identified on Figure 45 as the primary retail portion of Jasper Avenue. This area exhibits significant redevelopment potential capable of hosting new developments designed to facilitate high value retail. Furthermore the northern blocks on Jasper Avenue, between 108<sup>th</sup> and 106<sup>th</sup> Streets, are envisioned to be 100% redeveloped and almost 50% of the 101<sup>st</sup> to 105<sup>th</sup> street block will be redeveloped. Additional mandatory building setbacks in these areas will create a 9.0 metre sidewalk on the north side of the street which should be a location targeted for outdoor dining and cafes to enliven the street life. The goal of the following set of primary retail policies is to re-establish Jasper Avenue as a premier shopping destination serving the existing and emerging downtown population, workers and visitors. These policies aim to reinforce the development of a more interesting and continuous shopping experience along Jasper Avenue.

- a) Primary retail areas should generally be required to have minimum 80% ground floor frontage consisting of street-oriented retail, restaurants and /or commercial services along the Jasper Avenue block face.
- b) Retail uses should generally not exceed 930 square metres and occupy not more than 15 linear metres of street frontage
- c) Retail uses up to 4,645 square metres may be considered if the primary frontage on Jasper Avenue is more than 15 linear meters and portions of the use are located behind smaller retail units with direct frontage on the street or are stacked on multiple floors.
- d) Existing ground floor office uses are encouraged to integrate additional commercial retail frontage through retrofit and redevelopment.
- e) Residential, professional service and ground floor lobbies should be minimized in primary retail areas to maximize commercial retail frontage.
- f) Financial institutions are encouraged to locate on the mid-block portion of the street frontage. Corner locations are discouraged.
- g) Restaurants and pubs, excluding outdoor dining areas, should generally not exceed 465 square metres.
- h) Restaurants and outdoor dining establishments should be targeted to blocks between 108<sup>th</sup> and 105<sup>th</sup> streets.

## Commercial and Hospitality Zone

Within the eastern end of the study area, office and hospitality uses predominate and there are fewer redevelopment opportunities. This area offers less of an opportunity to create a continuous high value retail main street environment than areas identified as primary retail. Opportunities to enhance the commercial retail offering on Jasper Avenue east of 102<sup>nd</sup> Street generally include the insertion of new commercial uses within an existing structure. New commercial uses may be oriented toward daytime office workers or to support existing uses such as the Shaw Conference Centre, but may also include retail insertion within a financial institution or corporate office.

- a) Buildings within this area are encouraged to integrate additional commercial retail, services and restaurant uses frontage through retrofit and redevelopment
- b) Developments shall provide the opportunity for street fronting convenience retail and service uses to strengthen Jasper Avenue and abutting side streets as a pedestrian-orientated shopping area through the following:
  - i. Providing, for larger developments, over 1,860 square metres, and for smaller scale retail spaces to maintain a rhythm of fine-grained retail establishments at grade;
  - ii. All street level Commercial Uses that abut a public ROW, other than a lane, shall provide a primary direct access to the street;
- d) Residential, professional service and ground floor lobbies should be minimized in primary retail areas to maximize commercial retail frontage.

## Secondary Retail Zone

Secondary retail includes smaller scale boutique shops, cafes and other specialty retail. Secondary retail will continue to be focused on 104th Street and Rice Howard Way should emerge in the mews between Beatrice Carmichael Park and Veterans Park and facing the proposed Central Park.

- a) Secondary retail areas should generally be required to have minimum 80% ground floor frontage consisting of street-oriented retail, restaurants and/or commercial services along the block face.
- b) Existing ground floor office uses are encouraged to integrate additional commercial retail frontage through retrofit and redevelopment
- c) Residential, professional service and ground floor lobbies should be minimized in primary retail areas to maximize commercial retail frontage.
- d) Financial institutions are encouraged to locate on the mid-block portion of the street frontage. Corner locations are discouraged.
- e) Restaurants and pubs, excluding outdoor dining areas, should generally not exceed 279 square metres.

## Interior retail

Interior retail space houses some of the highest value retail in the downtown and over half of the approximately 167,225 square metres of its occupied retail space. Moving forward, the quantity of new interior-oriented retail space should be limited until street-related retail activity in the downtown is more healthy and vibrant.

- a) Buildings shall be designed to accommodate Commercial Uses to strengthen the pedestrian oriented shopping area through the following:
  - i. A minimum height of 4.5 metres for the ground storey is encouraged;
  - ii. Architectural treatment of new developments and substantial renovations shall have windows on the front façade of the building at each storey, and the placement and type of windows shall allow viewing into the building,
  - iii. Major shopping complexes and large-format stores over 2,000 square metres shall contain smaller scale retail spaces with direct access to the street to maintain a rhythm of fine-grained retail establishments at grade. All street level Commercial Uses that abut a street shall provide a primary direct access to the street,
  - iv. A minimum of 60% of street frontage for retail, services, and other commercial uses.

## Other retail

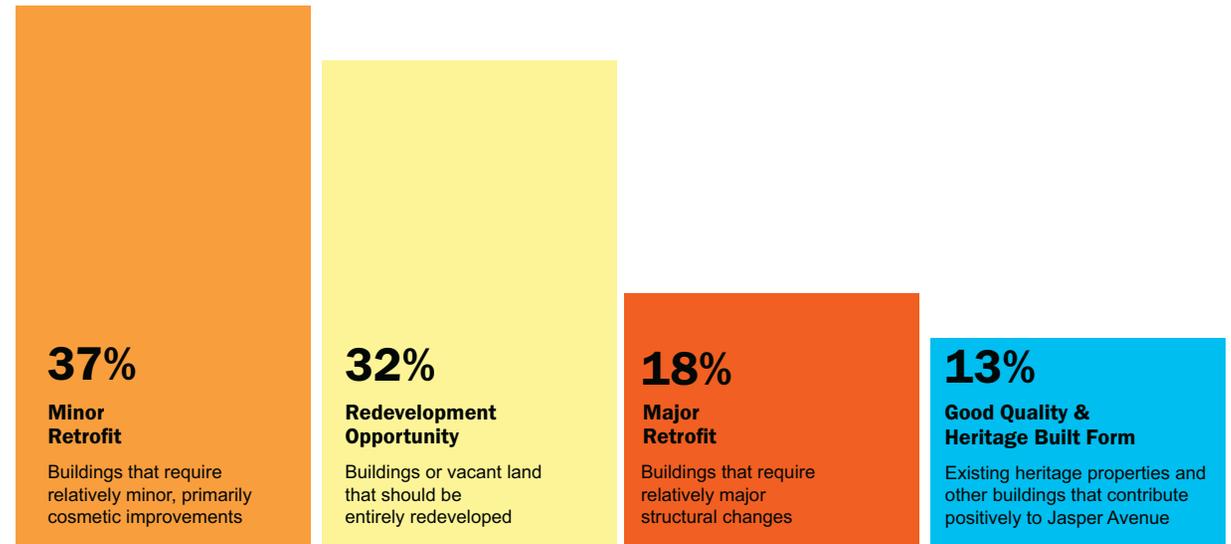
It is the intent of the proposed retail hierarchy to focus retail where it best serves to create a critical mass of activity. As such, outside of the Primary, Secondary and Interior Retail areas, the amount of retail should be limited. Within mixed use and residential areas, identified in Section 4.1, retail uses such as convenience stores, small food service to serve local residential and employment uses, as well as personal services is appropriate, but its quantity should be limited.



## 4.2 Urban Design Recommendations

Good urban design is fundamental to the creation of enduring, high value places. This section of the document sets out detailed urban design and built form recommendations to guide new development and the retrofit of existing structures to ensure that they contribute positively to an active, vibrant and visually appealing urban environment. Urban design policies will be utilized to structure and evaluate elements of the built environment to achieve design excellence.

The first part of this section presents a Built Form Improvement Strategy, which categorizes Jasper Avenue built form into one of four improvement categories and sets out a range of municipal actions aimed at improving the quality of the built environment on Jasper Avenue over the medium and long term. The second section outlines appropriate building typologies and detailed built form criteria to guide the overall Built Form Improvement Strategy, major and minor building retrofits, as well as new development projects within the study area. These recommendations should be reviewed by municipal staff for inclusion in the CCDPlan and proposed Downtown Zoning by-law.



▲ Figure 46  
Percentage of Each Built Form Improvement Category

### 4.2.1 Built Form Improvement Categories

Currently, building quality along the length of Jasper Avenue varies substantially. Through a detailed analysis of the urban design quality of buildings fronting Jasper Avenue, Figure 46 categorizes buildings into one of four improvement categories. Generally, the improvement categories indicate the level of change or improvement that is required to enhance the overall quality of the built environment and improve the physical character of Jasper Avenue. Please see Appendix A for additional detail.

#### Heritage and positively contributing built form

Approximately 13% of the street frontage is comprised of heritage buildings and other structures which contribute positively to the character and quality of place along Jasper Avenue and should be retained. Many of these are pre-war buildings, exhibit good street wall definition, interesting building articulation, visual rhythm and repetition of building elements, transparency at the

ground floor and utilize building materials, such as brick or stone, which have a sense of permanence and substance. Other buildings in this category include recently constructed buildings that exhibit many of the above listed characteristics, such as the new Sobeys at 104<sup>th</sup> Street. The following recommendations apply to heritage and positively contributing built form structures:

- Heritage buildings should be preserved through appropriate heritage designation, or through adaptive reuse and sensitive building additions, which retain the fundamental character elements of the structure.
- The City should promote public awareness of heritage and positively contributing built form through heritage designation plaques and architectural walking tours in conjunction with the Alberta Association of Architects.



### Redevelopment Opportunities

Approximately 32% of the street frontage has been identified as suitable for redevelopment. Much of the land in this category consists of surface parking lots, vacant parcels or buildings that are not historical or otherwise exhibit built form characteristics that merit retention over the long term. The following recommendations apply to heritage and positively contributing built form:

- a) All redevelopment is encouraged to adhere to the urban design guidelines contained in this document.

### Minor Retrofit

Approximately 37% of the street frontage is comprised of buildings that require minor retrofit to improve their ability to positively contribute to the street. The minor retrofit category contains structures which exhibit minor built form challenges particularly at street level, which could be remedied through primarily cosmetic changes that do not require major structural renovation to the building. Generally, improvements would include replacement of dark tinted or opaque windows, improvement of ground floor exterior façade, replacement of tired or deteriorated cladding materials, façade lighting, and the integration of private outdoor landscaping. These improvements are generally considered to be ‘good neighbour’ improvements and are relatively inexpensive in nature but can add a significant visual improvement to the street. The following recommendations apply to heritage and positively contributing built form:

- a) The minor retrofit of buildings should continue to be supported through the City of Edmonton’s Façade Improvement Program.
- b) Where possible, minor retrofits should adhere to the urban design guideline recommendations outlined in this document, in particular policies regarding signage, weather protection, transparency at grade, and others.

### Major Retrofit

Approximately 18% of the street frontage has been determined to comprise buildings requiring a major retrofit. These buildings exhibit a range of problematic built form attributes such as a lack of retail space on the ground floor, a ground floor which is either above or below the finished sidewalk elevation, few or no street entrances, bland facades which lack articulation and blank walls. Most of these buildings are substantial and may have another several decades before they reach the end of their life cycle and are replaced. Major retrofits may involve structural change, however will be an important element in improving the main street qualities of Jasper Avenue. The following recommendations apply to heritage and positively contributing built form:

- a) The City should consider encouraging the major retrofit of buildings through a matching grant program similar to the Façade Improvement Program.
- b) The City should enter into discussions and negotiations for any building seeking a development permit for a major retrofit structure on Jasper Avenue to encourage retrofit actions in conjunction with desired permit changes.
- c) Major Retrofits should adhere to the urban design guidelines as outlined in this document, in particular policies regarding façade articulation, entrances signage, weather protection, transparency and others.



▲ Figure 47  
Jasper Avenue Demonstration Rendering - North Side

## 4.2.2 Built Form Improvement Strategy

The following urban design recommendations are intended to be utilized by the private sector and the public sector in determining appropriate built form propositions for Jasper Avenue. These recommendations should be utilized to guide major and minor retrofit projects, as well as provide direction to all new developments to achieve a high quality of design excellence and guide city building in the area. Urban design guidelines are a tool to achieve a high quality physical environment while promoting a healthy range of architectural expression and variation. However, regulations alone cannot ensure high quality architecture and/or strong urban design. Successful

cities cultivate a culture of design excellence by leading with outstanding public sector projects, peer review processes such as design review panels like Edmonton Design Committee and executing national and international design competitions.

The following urban design guidelines provide specific directions to guide the evolution of new building development within the study area, and will inform building enhancements associated with minor and major building retrofits. The urban design guideline recommendations have been specifically developed for the Jasper Avenue New Vision study area, but could be reviewed and expanded should a broader

application be desired. Developers are encouraged to use the guidelines in formulating project propositions. The Edmonton Design Committee will also use these guidelines in formulating their expert opinion as to the relevant merits and success of individual projects before the committee.

### 4.2.2.1 General Site Development Urban Design Guidelines

General urban design guidelines provide clear direction for the building envelopes, orientation, setbacks, site landscaping, parking access, servicing and ground floor uses. General urban design guidelines ensure that buildings and their functional elements are situated such that they contribute positively to the quality of the public realm and pedestrian experience.



▲ Figure 48  
Jasper Avenue Demonstration Rendering

## General Site Development Urban Design

### Guidelines:

#### Building Envelopes, Orientation and Setbacks

- a) Buildings shall address the public street in order to clearly define the public realm, create a consistent street wall, and create attractive frontage supporting pedestrian use.
- b) Parking shall not be permitted between the edge of the public right-of-way and the building face on any public or private street.
- c) Buildings will occupy 100% of the street frontage on Jasper Avenue, excluding designated park frontage.
- d) Buildings will occupy 70% of the street frontage on side streets, except designated park frontage.

- e) A mandatory 0.0 metre setback from the public right of way is required for all buildings fronting Jasper Avenue, except in the blocks on the north side of Jasper Avenue between 105th and 108th Streets, which will exhibit a mandatory 3.0 metre mid-block setback, as illustrated in Figure 49, to support the emergence of an outdoor dining and restaurant district.
- f) A mandatory 0.0 metre side yard setback from the public right of way is required for all corner buildings with frontage on Jasper Avenue, excluding designated park frontage.

- g) Architectural features or foci are encouraged at all corner building locations to enhance the visual prominence and identity of the area. Buildings at corners shall provide courtyards, major entry ways or distinctive architectural features consistent with the style of the building or influences on the other corners of the intersection to enhance pedestrian circulation and, where applicable, enhance axial views.
- h) Pedestrian ‘through building’ connections are encouraged to link Jasper Avenue, to proposed new park north of Jasper Avenue for all blocks between 105<sup>th</sup> and 108<sup>th</sup> Streets .

### Site Landscaping

- a) The provision of private landscaping is encouraged at the interface between the building and the public ROW.
- b) Small scale visitor parking and passenger drop off and pick up areas shall be screened from the public street and located toward the rear of the building with a landscaped buffer.
- c) Service lanes that abut public parks should be treated as an extension of the park space and exhibit similar hard surface paving materials, pedestrian-scale lighting and low landscape materials defining it as a combined vehicular and pedestrian space.

### Vehicular Parking

- a) Parking that is part of the building podium shall be screened in a way that does not disrupt the continuity of the Street Wall and the character of the zone, and shall be limited to no more than 12 metres in height from grade. Screens may include, but are not limited to, public art and street fronting retail uses.
- b) Parkade(s) developed below grade shall be permitted to be built to the property line.
- c) Parkade(s) developed below grade on streets identified as neighbourhood streets in the Urban Design Framework for Downtown Streets within the Capital City Downtown Plan, Bylaw No. 15200 are

to be located sufficiently below grade, or a suitable alternative, to provide a minimum of 1.2 metre soil depth to contribute to healthy root development.

- d) No portion of an above grade parking garage on the ground (first) floor shall be allowed for a minimum depth of 8.0 metre from any front façade facing a public ROW, other than a lane. Vehicular access shall be from the abutting lane.
- e) Surface parking lots:
  - i. No surface parking shall be allowed, other than accessory parking that is located at the rear of a building and is accessed from the abutting alley;
  - ii. A minimum 4 metre landscaped setback shall be provided from any property line abutting a public ROW, other than a lane, for any surface parking area;
  - iii. The storage of materials inclusive of accumulated snow on non-accessory parking surface lots shall be in a location away from the public roadway to improve safety and visibility; and
  - iv. Lighting for the non-accessory surface parking lots shall be a minimum of 6 LUX.
- f) Structured above ground parking is encouraged to be located behind an active use, such that it is not visible from the public street.
- g) Structured above ground parking with street frontage should appear as an actively used building, with high quality exterior cladding and architectural detail.

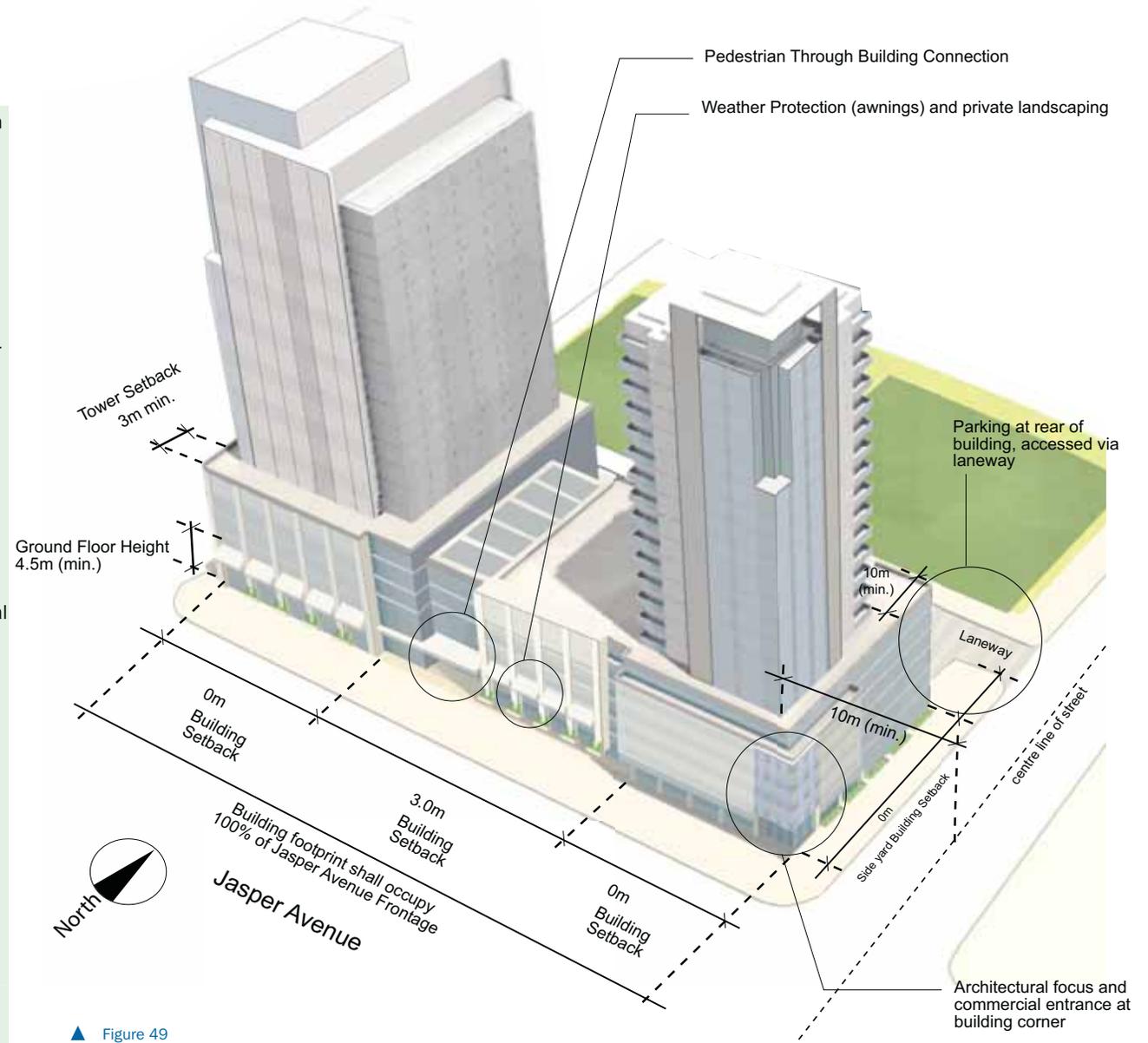
h) Parking facilities for bicycles should be provided in all new development.

- i) Private driveway access points shall not be permitted on Jasper Avenue
- j) Private driveway access should be provided from the rear lane or side street. No more than one point of access to the block should occur along the side street frontage on local streets.
- k) Loading, garbage storage and other services areas shall not be visible from any public street or any rear laneway abutting a public park. Screening in the form of fencing or opaque landscaping shall be provided for these areas in order to reduce their visual impact. Garbage receptacles/storage should be located within the building.

### Ground Floor Use

- a) Ground floor commercial/retail uses shall be required for all Primary and Secondary Retail frontage locations as identified in Figure 45.
- b) New professional and personal service uses and financial institutions shall not exceed 15% of the ground floor street frontage, on any block, within Primary and Secondary Retail areas identified in Figure 45. There is no restriction on these uses on the upper floors of buildings.
- c) All retail uses should have a clearly identifiable entrance on the street.

- d) All retail use building entrances should be flush with the public sidewalk.
- e) Residential uses shall not be permitted on the ground level along Jasper Avenue or any Primary or Secondary Retail frontages, as identified in Figure 49, but are permitted on other local streets. Residential lobbies are permitted, but should be minimized so as not to detract from the ground floor commercial uses.
- f) All residential uses at ground level will take their primary individual unit entrances from the street. Individual entrances will be elevated 0.3 meters above the street level, accessed by a walkway and front porch. Individual entrance features such as small wrought iron fenced gardens, landscaped trellis, and landscaping are encouraged. Residential uses above the ground floor should have a common access through a shared entry lobby.



▲ Figure 49  
General Site Development Urban Design Guidelines



#### 4.2.2.2 Building Heights and Massing

With the closing of the Edmonton City Centre Airport, the possibility to achieve greater building heights has emerged. The CCDPlan has established a maximum height regime for the entire downtown which anticipates the removal of the Airport Height Control Overlay covering portions of the downtown due to the airport closure. There are however, many existing structures within the downtown which have not reached the end of their life cycle and will be in place for some time. In addition, there are many small-sized parcels which are functionally too small for tall buildings and require larger land assemblies in order to be an appropriate and realistic redevelopment site for a tall building that can reflect good urban design qualities. The downtown will only reach its maximum height incrementally through land assembly and redevelopment over the long term. There is a need to carefully guide development on those sites which today are sufficiently sized to accommodate a tall building, as well as guide development on smaller sites not appropriate for tall buildings, but where there may be interest in redevelopment, addition or expansion.



#### 4.2.2.3 Tall Buildings

As noted above, not every site is appropriate for a tall building. For instance, when tall buildings are constructed too close together, resulting wind conditions, lack of access to sunlight and blockage of sky views can create uncomfortable pedestrian environments. In addition, when buildings are constructed too close to the side property line, the development of one site can restrict or prohibit adjacent sites to develop in a similar manner.

The following recommendations has suggested specific podium and tower configuration for tall buildings, which will occur for most development sites on Jasper Avenue and the adjacent context. This building typology creates a sense of human scale and good street definition along the building base and allows for taller slender tower components to be stepped back on top of the podium, reducing the impacts of height at the pedestrian level. This form also optimizes sunlight penetration to the ground level and preserves sky views. Tall buildings are considered those over 26 metres tall. The following guideline recommendations will regulate tall building location, form and relationships to their surroundings.

- a) Tall buildings are appropriate for a building site that is not smaller than 33 metres deep by 50 metres wide or 43 metres deep by 40 metres wide
- b) Podium height should be 4 to 6 storeys, west of 102nd Street; and 6 to 9 storeys, east of 102nd Street.
- c) Podiums should be massed to reflect a series of linked smaller buildings by incorporating recessed or setback building segments occurring at intervals along the street frontage. Long, continuous facades along a single building plane should be avoided.
- d) The tower component of the building above the podium should be stepped back a minimum of 3 metres from the front of the podium face; 10 metres from the rear property line; 10 metres from both side property lines in the case of a mid block buildings, and a minimum 10 metres from the centre line of the adjacent side street in the case of a corner building condition (Figure 49).

- e) The minimum space between non-residential Towers shall be 20 metres. The minimum space between a residential Tower and any other Tower shall be 25 metres. The Development Officer may vary the Tower spacing in consideration of the following:
  - i. The visual sun/shadowing, and other microclimatic impacts on adjacent residential development; and
  - ii. The recommendations, and mitigative measures specified in any required technical studies.
- f) Above the building podium, the tower component of office buildings should generally have a maximum floor plate area of 2,000 square metres. Residential and hotel tower components should have a maximum floor plate area of 750 square metres.
- g) Step-backs and reduction of building mass on higher floors is encouraged to reduce the visual impact of taller buildings.
- h) Tall buildings should be designed and oriented to minimize shadow impacts on all parks and open spaces. Shadow impact studies should be included as part of development applications.

- i) New development should not visually impede the setting of listed designated heritage buildings. Where heritage buildings are low-scaled, the podium of the tall building should immediately adjacent to the heritage building should be set back from the property line and be approximately equal in height to the heritage structure. Building materials which complement the adjacent historic buildings, while remaining distinguishable as a contemporary structure, is also encouraged. Historicist architectural style should be discouraged.
- j) Given the importance of office uses in the downtown, the intent of the above policies should be followed but are not mandatory for office uses. Office uses are permitted to exclude the podium if it can be demonstrated that the tower component exhibits design excellence in engaging the public realm. Careful consideration needs to be paid to the design of lower floors, including the use of canopies and other design elements to create a sense of human scale.



#### 4.2.2.4 Mid-rise building policies

Mid-rise buildings are a desirable building type for Jasper Avenue and the adjacent context. Mid rise buildings are appropriate for sites which are too small for tall buildings and would not permit the required tower setbacks from the side, front and rear. Mid-rise buildings are considered those between 4 and 9 storeys tall.

- a) Mid-rise buildings should be built at and along the full extent of all street frontages
- b) Mid rise buildings should generally be 4 to 6 storeys west of 102<sup>nd</sup> Street; and 6 to 9 storeys east of 102<sup>nd</sup> Street
- c) Mid rise buildings over 6 storeys should include a 3.0 metre building step back from all street frontages at the 7th storey
- d) Mid rise buildings should be massed to reflect a series of linked smaller buildings by incorporating recessed or setback building segments occurring at intervals along the street frontage. Long, continuous facades with a single building plane should be avoided.



#### 4.2.2.5 Building Elements

The design of the various building elements will play a substantial role in the overall quality of the build form and their ability to create an active and pedestrian-friendly street. Well designed buildings will differentiate Jasper Avenue and contribute to the vitality of the area. While buildings are permitted to line most of the street frontage, differential treatment of primary building elements will ensure that large redevelopment parcels will express a range of building treatments and hierarchical elements, lending interest and variation to the built environment. The following building element guidelines will ensure the evolution of an attractive and interesting urban environment.

##### Building Base – Ground floor

- a) On Jasper Avenue and Secondary Retail frontages (Figure 49), the minimum ground floor height should be 4.5 metres.
- b) 2 storey commercial retail units are permitted on Primary and Secondary Retail frontages.
- c) The building base should have well articulated street level façades with high quality materials to create a visually interesting street interface and sense of human scale.



- d) On Jasper Avenue, retail and commercial uses should comprise at least 80% of the ground floor building frontage. Active commercial uses facing park spaces are encouraged where possible.
- e) A minimum 80% of ground floor frontage in Primary Retail areas (Figure 45) should consist of transparent glazing.
- f) Building and commercial entrances should be easily identifiable with direct access to the street or street corner.
- g) Large entry lobby frontage, blank walls, servicing and other inactive functions should be prohibited along Jasper Avenue.
- h) The entrance to residential building lobbies should be limited to no more than a 7.5 metre width at on Primary or Secondary Retail areas (Figure 45).
- i) Weather protection in the form of awnings is encouraged. Awnings should consist of transparent glass material at a minimum height of 4 metres above ground. Arcades and colonnades should be prohibited.
- j) Awnings should be allowed to encroach within the public ROW.



- k) The building base should have high-quality building materials, such as stone or brick. Changes in building material should occur above the ground or second floor and above the podium levels.
- l) Pedestrian through building connections should be marked with a prominent entry feature such as a two story archway and may be fully enclosed but publicly accessible.

**Building Mid Section – Podium and Mid-Rise Building Elements**

- a) An 8.0 metres to 12.0 metres interval of vertical articulation is recommended to add visual interest along the street frontage.
- b) The mid-rise building facade should be well articulated with an interplay of rhythm between transparent glass and solid materials.
- c) The horizontal and vertical alignment of windows should be generally consistent while creating variation and interest along the building façade.
- d) Balconies should be designed as integral components of the building and not appear as ‘tacked on’. Projected balconies on the building mid section should not be permitted. Balconies should occur in recessed building segments.



- e) Step backs, balconies and building fenestration should emphasize vertical proportion and deemphasize mass.
- f) The use of brick or stone materials is encouraged.
- g) Mirrored or brightly coloured glazing should not be permitted.
- h) A distinctive cornice element is encouraged at the top of the podium level.

**Tower Elements**

- a) Tower portions should have windows on all sides of the building. Blank walls are discouraged.
- b) Towers should have high quality building materials, but should generally have a higher proportion of glazing than the podium level.
- c) Balconies should be designed as integral components of the building.
- d) Upper floors of towers are encouraged to have step backs and reductions in floor plates to provide visual interest and reinforce the slender appearance of towers.
- e) At the top of towers, mechanical units and elevator cores should be screened or integrated into the design of the building and contribute to an attractive skyline profile.



**Building Materials and Quality**

- a) Important civic, cultural or institutional buildings are encouraged to undergo design competitions and distinguish themselves with high quality architecture befitting their special status in the community.
- b) Buildings should be of high quality and include durable and attractive building materials such as stone and brick. Low quality and poor wearing materials such as stucco, vinyl siding and extruded insulation and finish systems (EIFS) should be prohibited.

**Signage and Architectural Lighting**

- a) Signage should conform to the Downtown Zoning By-law.
- b) Signage should help provide identity to individual business and designed in a manner that adds visual interest to the public realm.
- c) Architectural lighting is encouraged for existing heritage buildings and new developments. Lighting specialists should be retained to design a low voltage up lighting strategy to light the mid building segment, which will reinforce the verticality of the structure and enhance the character of the main street environment.



#### 4.2.2.6 Environmental Site and Building Design

Sustainability is inherent to downtowns. Downtowns have increased density, contribute to the reurbanization and reuse of lands within the existing built-up area, reduce the need for automobile ownership and overall automobile trips and make more efficient use of resources. Site and building elements should be designed to achieve high standards of sustainability.

Development within the study area should be encouraged to incorporate sustainable site and building practices including:

- a) Optimizing energy efficiency of buildings;
- b) Pursuing LEED™ certification or comparable standards for all new buildings;
- c) Incorporating green roofs;
- d) Using renewable energy sources;
- e) Using innovative wastewater technologies;

- f) Introducing innovative stormwater management techniques, including reduction in quantity, bioswales; permeable surfaces; water collection, filtering and reuse;
- g) Integrating indigenous planting and biodiversity of material within landscaping, streetscaping and public spaces;
- h) Incorporating green roofs;
- i) Provision of internal building recycling receptacles;
- j) Orientation of buildings to optimize solar gain.

## 4.3 Mobility Recommendations

### 4.3.1 Prioritizing Modes of Transportation

The New Vision is fundamentally about making Jasper Avenue a comfortable and attractive place for pedestrians. The rebalancing of the Jasper Avenue right of way to dedicate more space to pedestrians is the fundamental move to achieve this principle.

- a) The design and function of the mobility network should recognize pedestrian circulation and comfort as its highest priority, with cycling as well as transit circulation as other high priorities.
- b) Vehicular movement should function effectively and smoothly, but not unduly impede the comfort and safety of pedestrian and cycling modes.

### 4.3.2 Public Transit

Jasper Avenue is a critical street for the function of public transit in the downtown and for the entire city. A large number of bus routes run along Jasper Avenue and the LRT line runs beneath the street.

- a) The reconfiguration of the streetscape should effectively accommodate transit vehicles including provision for lay-bys at 101<sup>st</sup> and 100a Street eastbound and 101<sup>st</sup> and 102<sup>nd</sup> Street westbound.

- b) The location of transit shelters should not impede pedestrian movement on the street. Refer to the Streetscape Concept Plan in Section 3.4 for further guidance.
- c) Transit stations and stops should be priority areas to incorporate weather protection and passenger information systems.

### LRT Entrances

The design of LRT station entries is a particular concern in ensuring a high quality public realm and sense of place on Jasper Avenue. Over time, the relocation and/or replacement of LRT station entries should consider the following:

- a) Locate within adjacent buildings where feasible.
- b) Keep entrances clear of pedestrian paths.
- c) Use transparent glazing.
- d) Make entrances distinct architectural features of the streetscape.

### 4.3.3 Cycling

Jasper Avenue is not a designated cycling route, however it will accommodate cyclists through a wide curb lane as illustrated in Figures 20 and 21. Cyclists will also share the road network with motorists on roadways that are better balanced and have slower moving traffic. Bicycle parking is also an important consideration as Jasper Avenue transforms.

- a) The reconfiguration of Jasper Avenue should facilitate safe movement by cyclists.
- b) The Jasper Avenue streetscape should include on-street bicycle parking posts, located in a manner that does not obstruct pedestrian movement or transit functions.
- c) For non-Residential Uses with a gross floor area over 5,000 square metres change rooms, showers, and covered, secured parking areas shall be provided to the satisfaction of the Development Officer. LEED™ standards shall be utilized to calculate the required areas.

### 4.3.4 Laneways and Servicing

Servicing functions should be accommodated in a way that reduces pedestrian and vehicular conflict and is unobtrusive to the public realm. Generally, servicing functions are a deterrent to a pedestrian-oriented environment. Such functions should be screened from pedestrian view, and not located on Jasper Avenue. Some laneways will serve dual functions as a mews street, which will include active uses and will be an important component of the public realm system.

- a) Servicing and vehicular parking generally should be accessed from rear laneways.
- b) Servicing areas for waste and recycling pick up, delivery and other building functions should be located to the rear of buildings within laneways or internal to blocks to ensure that servicing functions do not interfere with movements on streets or the quality of the public realm.
- c) Mews, including those facing the proposed Central Park and between Veterans Park and Beatrice Carmichael Park should be designed as multi-use hard landscaped environments for safe and comfortable use by pedestrians as well as service vehicles.

### 4.3.5 Parking Management

Parking should be designed and located in a manner that reduces pedestrian and vehicular conflict and which will not diminish the quality of the public realm. Currently, downtown parking is located in a variety of surface parking lots and private parkades. Moving forward, a shift in the thinking about how parking is mandated, managed and designed is needed. Relocating parking to underground and/or select above-ground parking garages will improve the visual character of Jasper Avenue, as well as create new development opportunities on sites that are currently surface parking. The advent of municipally managed parking would allow the public to better control the parking supply and cost, encouraging privately-held surface parking lots to be redeveloped over the short term.

- a) Driveway access to private parking shall not be permitted along Jasper Avenue.
- b) The City should prepare a new comprehensive Downtown Parking Strategy that addresses the supply, location and management of off-street and on-street parking, including: building of shared or common parking facilities; and the creation of

a municipal parking authority to manage parking provisions and provide shared parking facilities within downtown.

- c) A development levy and/or cash-in-lieu policy to help finance parking structures should be explored.
- d) Stand alone parkades should be prohibited on lands fronting Jasper Avenue
- e) To eliminate the potential for new surface parking, existing buildings should not be permitted to be demolished prior to the approval of a development permit for a new use.



▲ Example of a screened parking garage

### 4.3.6 Universal Access

By virtue of their immediate proximity to transit and a wide array of amenities, downtowns are communities that attract a number of people with limited mobility. As such, the design of mobility systems must ensure universal accessibility so that all people can comfortably and safely move throughout the area.

- a) All curbs at crosswalks and intersections should have curb cuts to accommodate people with limited mobility.
- b) All transit stations and stops should provide universal wheelchair accessibility.
- c) Signalized intersections and cross walks should include visual and audible aids.
- d) The design of sidewalks should include an urban Braille system to assist the visually impaired.



▲ Curb cuts



▲ Universally accessible parks



▲ Braille system for assisting the visually impaired

Figure 50  
Illustrative rendering of the revitalization of Jasper Avenue



# Part Five: Implementation

*To achieve the New Vision for Jasper Avenue and enable the street and surrounding environment to reach full potential, deliberate action, leadership, cooperation and investment are required.*

## 5.1 Policy and Zoning Review

This section of the document outlines a number of key implementation considerations to bring the New Vision forward. These include recommended policy changes, a range of recommended future projects and studies, options for funding various initiatives and strategies aimed at parkland acquisition and public realm development, parking management, as well as other tools to implement elements of the New Vision.

### 5.1.1 Integrate New Vision policies in the Capital City City Downtown Plan (CCDPlan)

The CCDPlan will act as the statutory document regulating growth and development for the downtown. The New Vision is not a statutory document; therefore its policies are not in-force and can only be effectively implemented if embedded in the CCDPlan. It is recommended that the City of Edmonton incorporate and embed the land use, urban design and other policies and recommendations contained within the New Vision for Jasper Avenue document in the CCDPlan.

### 5.1.2 Integrate New Vision policies in the Zoning By-law

Similar to the above recommendation regarding the CCDPlan, the Downtown Zoning By-law will be one of the primary vehicles through which urban design policies will be enforced. It is recommended that the City of Edmonton incorporate and embed the land use, urban design and other policies and recommendations contained within the New Vision for Jasper Avenue document in the Downtown Zoning By-law.

## 5.2 Strategic Short Term Capital Improvement Projects: 0- 5 years

The New Vision initiative highlights a number of strategic projects that should be undertaken and implemented by the City of Edmonton over the next 25 years. This includes the implementation of the new Jasper Avenue Streetscape Concept, as well as a range of public realm improvements and other municipal improvement programs. It is recommended that some of the projects be implemented in the short term to act as catalysts for growth and continued private sector reinvestment in the downtown. Some of the initiatives can be undertaken independently, while others should be timed and phased to occur logically with other projects should happen concurrently with private development interests. The recommended timing and phasing of capital improvement projects are discussed below.

### 5.2.1 Implement the New Vision Jasper Avenue Streetscape Improvement.

The Jasper Avenue New Vision initiative has been identified as Council's first priority initiative in the new CCDPlan. The renewal and revitalization of the Jasper Avenue Streetscape Concept is the central catalytic project identified within the New Vision Initiative, aimed at kick starting a comprehensive program of focused city building in the downtown. The Streetscape Concept Plan, as outlined in Section 3.4 of this report, will proceed to detailed design and costing by early 2011 and by the foundation for the preparation of construction and tender documents to implement the streetscape improvement in a phased construction process.

Several options for the funding of the proposed streetscape improvement were explored and evaluated over the course of the New Vision initiative. Density bonusing and the use of redevelopment levies were considered and dismissed as potential funding mechanisms for the proposed streetscape improvement projects, including lane reduction and sidewalk widening. It was felt that density bonusing and

the use of levies cannot be achieved on a piecemeal or incremental basis and requires a comprehensive and carefully phased construction process to minimize disruptions to property owners and businesses on Jasper Avenue. Redevelopment levies are, however, viable funding options for various other public realm initiatives highlighted in this study and are discussed latter in this section. A Community Revitalization Levy (CRL) within the New Vision study area was considered and also deemed inappropriate because the levy area is too small, however, this financing mechanism could be considered on a wider basis for the CCDPlan area to fund a wide range of public realm improvements, including parkland acquisition and other streetscape improvements.

A local improvement by-law was utilized to finance the previous Jasper Avenue streetscape improvement and currently, property owners on portions of Jasper Avenue are still contributing to this improvement by-law. Therefore, it is unlikely that an additional local improvement by-law would be well received by

current property owners or those still contributing to the last streetscape improvement effort. In addition, the improvement of Jasper Avenue is not a local improvement matter, but clearly an initiative of city wide importance.

It is recommended that the City of Edmonton finance 100% of the Jasper Avenue streetscape improvement. This will:

1. Demonstrate municipal commitment to the renewal and revitalization of Jasper Avenue as an important city-wide priority;
2. Demonstrate municipal commitment to design excellence in the downtown and the quality of design expected of the private sector;
3. Create a catalyst for continued public and private sector re-investment and city building in the downtown. The downtown covers less than 1% of the City's land area but generates 6.8% of the City's property tax base and 10.7% of the City's business tax base. In 2007, the downtown provided

the City with over \$64 million in tax revenues. Reinvestment in the downtown area will contribute to economic and cultural prosperity by creating jobs, encouraging small business to develop, and creating a more rich and inviting experience for all Edmontonians. Investment in the Jasper Avenue streetscape initiative can be a catalyst for private sector reinvestment in the downtown resulting in the potential for:

- 771,169 new residential gross square metres;
  - 15,000 new residents;
  - 109,017 new commercial gross square metres;
  - 60,990 new retail gross square metres and yielding an estimated annual tax revenue increase of \$19 million.
4. Minimize disruption to local businesses and downtown users by allocating funds for comprehensive block by block streetscape construction in a coordinated phased construction program.

### **5.2.2 Designate and acquire the proposed park located at Jasper Avenue and 100th Street adjacent to Hotel Macdonald.**

As highlighted in Section 2.2, there is considerable park land deficiency within the downtown, which must be addressed if the downtown is to evolve as a healthy and desirable mixed use community and as a home to more residents. The New Vision initiative has identified several strategic locations for the development of new park space, discussed below, but the open space adjacent to Hotel Macdonald, should be viewed as a priority park site.

It is recommended that the City take steps to acquire the open space at Jasper Avenue and 100th Street adjacent to Hotel Macdonald through purchase, transfer of development rights, land swaps, the disposal of surplus municipal lands or through other means presented in funding tools latter in this section and designate the space as a public park.

### **5.2.3 Designate and acquire other park spaces within the Jasper Avenue New Vision Study area.**

The New Vision recommends a series of potential locations for substantial new park development, including within the Warehouse District on lands which are primarily vacant or contain a scattering of one and two story buildings. Other new parks are proposed through future development sites to link Veterans Park to Beatrice Carmichael Park and 105<sup>th</sup> Street. In order to address park space deficiency in the downtown, and create a setting for new mixed use development and investment, the City must pursue an aggressive parks acquisition strategy to acquire new open spaces before opportunities are lost through site redevelopment.

It is recommended that the City designate lands identified in the New Vision as public parks and take steps to acquire the land through purchase, transfer of development rights, development agreements, partnerships, land swaps, the disposal of surplus municipal lands and through other means presented in funding tools in this section of the document.

### **5.2.4 Undertake a Design Competition for Beaver Hills House Park**

Beaver Hills House Park is currently the only significant public park fronting Jasper Avenue. As such, its potential as an important urban space should be fully realized. The current design does not effectively support urban activity and it is in need of rejuvenation to function as a central gathering space on the Avenue. Design competitions are an effective way to solicit highly innovative design talent with outcomes that permit the community and city to engage in an important dialogue about the role, function, character and use of public spaces. Given Beaver Hills House Park's critical role on Jasper Avenue and its need for rejuvenation, it is a logical candidate for a national and international design competition.

It is recommended that the City undertake a two part design competition for Beaver Hills House Park involving the issuance of a Request for Qualifications to create a shortlist of not more than five design teams to proceed to a paid design competition for the park. The City should select a panel of Landscape Architects, Architects and Urban Designers to jury the competition. The winning team would enter into a contract with the City to undertake detailed design development and the tender document package. To ensure high quality design response, the City should compensate shortlisted competitors in the range of \$35,000 to \$50,000 each.

## 5.3 Future Studies

Throughout the Jasper Avenue New Vision several additional studies are recommended to continue the process of focused city building in the downtown. The following summarizes the recommended studies.



### 5.3.1 Streetscape and Public Realm Improvement Studies

There are number of additional studies and projects that are recommended to help create a complete and connected public realm network surrounding Jasper Avenue. These studies include:

- **99th and 100th Streets Streetscape Improvement Study**
- **100a Street and 101a Avenue (Rice Howard Way) Streetscape Improvement Study**
- **College Walk Public Realm Improvement Study**

This study should be coordinated with the 100a Street and 101a Avenue (Rice Howard Way) Streetscape Improvement Study.

- **River front Heritage Trail Completion Study**

It is recommended that the City of Edmonton plan for and integrate the streetscape and public realm improvement studies within operating and capital budgets and explore a range of strategies to fund implementation of study recommendations.

### 5.3.2 Downtown Municipal Parking Strategy

The quality, quantity and management of parking resources are important to the revitalization of Jasper Avenue and the downtown. Many large cities utilize a parking authority to effectively manage the quantity and cost of short term and long term parking, encouraging private sector surface lots to redevelop. The development of municipal parking is often financed partially through cash-in-lieu policies whereby a reduced parking requirement is mandated for new development in exchange for cash-in-lieu payments to the municipal parking authority that in turn develops publicly accessible off-site parking facilities.

In addition, inappropriate parking requirements can limit revitalization efforts and serve to encourage automobile dependence in downtown environments. The CCDPlan supports the overall reduction of automobile dependence, yet, parking standards are generally uniform across the municipality and do not reflect the true mobility patterns of a downtown urban context with a higher order transit system. Standard parking requirements in a downtown context can limit the amount of retail and commercial development

as it can be expensive to accommodate parking requirements in areas of high land value after resulting in the construction of expensive structured parking. Reduced parking standards in downtowns is a common policy direction in most North American cities.

It is recommended that the City undertake a Downtown Parking Strategy which includes exploring the creation of a Municipal Parking Authority, the creation of shared public parking facilities in the downtown, and developing appropriate or reduced parking standards for new development to better manage the supply, location and form of parking in downtown, encourage active transportation and transit use and remove barriers to downtown reinvestment.

### **5.3.3 Undertake comprehensive urban design studies for areas north and south of the Jasper Avenue New Vision study area to create a cohesive physical vision for the entire downtown.**

As part of the CCDPlan the Jasper Avenue New Vision was the first of five recommended urban design plans. In addition, there are other municipal and provincially initiated public realm projects underway in the downtown, including the 108th Street Capital Boulevard Plan and 104th Street Streetscape Initiative. Collectively, these initiatives provide strong guidance and direction for the continuation of similar 'New Vision Initiative Studies' in the remaining areas of the downtown.

It is recommended that the municipality undertake urban design studies for the areas north and south of the Jasper Avenue New Vision study area to create a cohesive physical vision for the entire downtown, address the need for a connected network of parks, streetscapes and reinforce the urban character of the downtown. Key policy and urban design recommendations of the New Vision for Jasper Avenue should be evaluated regarding their suitability to be translated to other parts of downtown.

### **5.3.4 Community Facilities Needs Assessment Study**

The New Vision demonstrates the potential for significant new residential and commercial development within the study area. To ensure that the needs of this future working and living population are met, it is important to understand the range of community facilities and services that should be planned for including a timeline for the development of these facilities.

It is recommended that the municipality undertake a Community Facilities Needs Assessment Study to plan for the needs of the future population in downtown.

## 5.4 Other Revitalization Mechanisms and Recommendations

### 5.4.1 Management and Programming of Jasper Avenue

Management and programming of the public space on and around Jasper Avenue is critical to making the experience of the street attractive to people. This includes hosting special events and festivals, as well as providing permit access to groups for temporary street closures, or to permit the use of portions of the street and /or Beaver Hills House for special events for seasonal commercial kiosks.

It is recommended that the City of Edmonton and the Downtown Business Improvement Association explore opportunities to actively program Jasper Avenue and adjacent open spaces throughout the year considering the type and nature of festivals. The development of policy and permitting processes regarding the introduction of seasonal commercial kiosks, street performers and temporary street closures for special events should be explored.

### 5.4.2 Façade Improvement Program

The Façade Improvement Program has served as a successful financial tool to enhance street front retail environments. The New Vision concludes that a significant portion of building frontage on Jasper Avenue requires relatively minor improvements to facades to more positively contribute to the quality of the street. These were categorized as 'minor retrofits,' as explored in Section x, and are appropriate revitalization projects that could be facilitated through the Façade Improvement Program. Buildings requiring more significant structural renovation, categorized as 'major retrofits,' as discussed in Section x, would need significantly more investment to improve their urban design characteristics.

It is recommended that, due to the success of the program in the past, the City of Edmonton should continue to offer and potentially increase financial contributions associated with the Façade Improvement Program. The City should engage in discussions with property owners of minor retrofit properties to encourage their participation in this important program. It is also recommended that the City consider implementing a major retrofit Façade Improvement Program to encourage more costly or substantial improvements which are important to enhancing the quality of Jasper Avenue. The recommendations of the New Vision's Built Form Improvement Strategy presented in Appendix A should be pursued for all major and minor retrofit improvements participating in the Façade Improvement Program on Jasper Avenue.

### **5.4.3 Establish a Jasper Avenue Merchandizing Coordinator**

As outlined in Section 4.1, there is value in proactively managing the retail and shopping mix on Jasper Avenue to attract a range of national clothing retailers, independent specialty and boutique retailers, restaurants and needs based shopping outlets such as drug stores. A concerted effort is needed to identify and recruit the type of retailers that would be attracted to a downtown urban main street. The City and Downtown Business Improvement Association should work in partnership to establish a merchandizing coordinator that would work with developers and property managers to identify and recruit high value retailers to Jasper Avenue.

It is recommended that The City of Edmonton establish a new Merchandizing Coordinator position in consultation with the Downtown Business Improvement Association to work with property owners and developers to help attract a suitable retail mix for Jasper Avenue.

### **5.4.4 Utilize incentives to increase the permanent residential population of downtown**

The ultimate success of Jasper Avenue will rely on a significant increase in the downtown residential population. The New Vision sets out a framework for city-building on and around Jasper Avenue that can accommodate thousands of new residential units. Downtown residential development could be significantly bolstered by growth management policies limiting outward city growth and focusing reinvestment and redevelopment within the existing built-up area. In the absence of aggressive action, development incentives may be needed to encourage investment and redevelopment in the downtown until such a time that market demand shifts toward downtown living. Previous redevelopment incentives have been used with relative success in Edmonton.

It is recommended that the City of Edmonton re-establish a program of residential unit development incentives to prioritize downtown development and the revitalization of Jasper Avenue.

### **5.4.5 Communicate the positive benefits of a focused program of city-building in the downtown.**

The New Vision's ultimate implementation will rely both on public and private sector investment. The City should lead the effort with funding and implementing the Jasper Avenue Streetscape Concept Plan initiative, as well as other public realm and parks acquisition activities. The development community understands that these public benefits have a very real private benefit as well. The better, more interesting and vibrant a downtown provides for a greater the demand for development of residential and commercial space. At the same time, developers have a responsibility to respond and contribute to these public benefits through financial contributions and improved design quality.

It is recommended that the City of Edmonton develop a communications strategy to convey the benefits, roles and responsibilities of various actors that will implement the New Vision.

## 5.5 Funding Tools

This section sets out a number of tools and strategies that may be utilized by the City to fund and achieve the various improvements outlined in the New Vision.

### 5.5.1 Development Agreements

In order to obtain some of the recommended public realm initiatives such as a major new downtown park space, development agreements, transfers of development rights and land swapping may be necessary.

It is recommended that the City should examine the possibility of creating development agreements with property owners, with the goal of achieving the public realm initiatives and park acquisitions recommended in the New Vision.

### 5.5.2 Community Revitalization Levy

A Community Revitalization Levy (CRL) is a variation on the Tax Increment Financing (TIF) tool which is commonly used in the United States, as well as some municipalities in Canada, and has recently been adopted in Alberta under the Municipal Government Act. It is being utilized in the City of Calgary's Rivers District and is being implemented for the Fort Road area and the Quarters Downtown in Edmonton. This financing tool works by identifying a redevelopment area with an established boundary. Within that boundary, the existing tax base that will continue to flow into the City's general revenue stream is determined. Through the Community Revitalization Levy Agreement, the City or a development agency borrows money, which is invested in affordable housing, public realm improvements and other hard and soft infrastructure. This investment spurs private development and results in an expanded tax base. The incremental amount above the 'initial' existing tax base is used to pay back the loan.

It is recommended that the City of Edmonton consider a Community Revitalizations Levy for the CCDPlan area to advance the acquisition of parks, public realm improvements and other infrastructure projects within the New Vision study area and throughout downtown.



## **Appendix A**

Existing Street Analysis and  
Recommendations



# Existing Street Analysis and Recommendations

## 18% Major Retrofit

Buildings that require relatively major structural changes



Currently, building quality along Jasper Avenue varies substantially. Figure 1 categorizes building frontage along Jasper Avenue into one of four categories. These categories indicate the level of change necessary to elevate the urban design performance of the building to a desirable standard. The goal of the retrofit of buildings and redevelopment of new buildings is to create a high quality interface with the public realm and a resulting built form quality which is capable of facilitating high value retail and

## 37% Minor Retrofit

Buildings that require relatively minor, primarily cosmetic improvements



commercial uses, which will contribute to street life and vitality on Jasper Avenue. This goal is particularly important for those buildings located in the Primary Retail area of Jasper Avenue, west of 102nd Street.

The following Built Form Improvement Strategy documents the built form quality along both sides of Jasper Avenue from 111 Street and 97 Street and makes specific recommendations for improving the quality of the environment over time. All redevelopment sites, major retrofit and

## 13% Good Quality & Heritage Built Form

Existing heritage properties and other buildings that contribute positively to Jasper Avenue

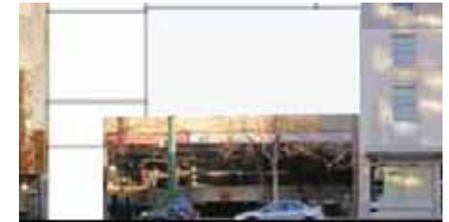


minor retrofit conditions should adhere to the urban design recommendations contained in Part Four of this document, Policy, Urban Design and Other Action Strategy.

In addition, the municipality should refer to the Built Form Improvement Strategy and its recommendations when dealing with development applications with frontage on Jasper Avenue within the study area.

## 32% (Re)Development Opportunity

*Buildings or vacant land that should be entirely redeveloped*



## North Side: Blocks 111-109



### Site 1

- Building appears dated and unattractive
- Current active ground floor uses
- Interface is not conducive to a high value retail street
- Ground floor podium height is too low, resulting in a lack of an urban scale street wall

#### Recommendations - Major Retrofit

- Encourage two storey ground floor retail height to accommodate higher value retail
- Update facade materials to include a greater proportion of transparent, non-tinted glazing

- Bring ground floor uses to street level
- Incorporate unobtrusive or transparent weather protection (awnings or canopies)

### Site 2

- Interface is not conducive to a high value retail street

#### Recommendations - Major Retrofit

- Redevelop ground floor to include commercial uses flanking hotel lobby
- Update facade materials to include a greater proportion of transparent, non-tinted glazing
- Incorporate unobtrusive or transparent weather protection (awnings or canopies)

### Site 3

- The design, scale and use contributes to a suburban feel to the street
- Existing buildings should be viewed as temporary and be comprehensively redeveloped to an urban scale and use with the balance of the block, which primarily consists of surface parking

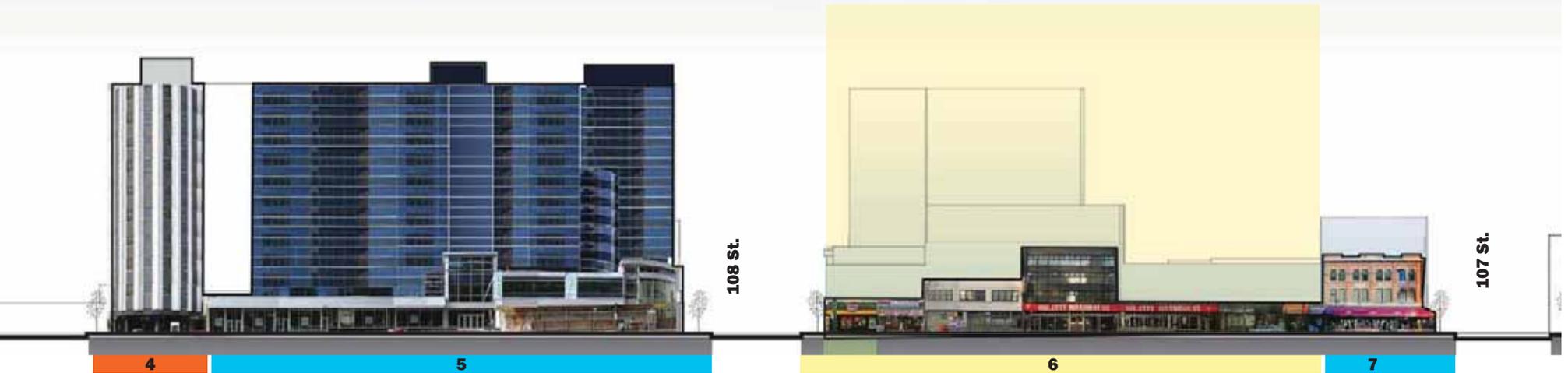
#### Recommendations - Redevelopment

- Redevelop site per urban design guidelines to include higher density and mixed use

### Site 4

- Tinted glass at grade reduces interaction between the inside retail use and the public realm
- Facade is generally dated looking
- Lack of definition between retail uses on ground floor and floors above
- Active retail use at grade

## North Side: Blocks 109-107



### Recommendations - Major Retrofit

- Encourage two storey ground floor retail height to accommodate higher value retail
- Replace dark tinted glass at grade with transparent, non-tinted glazing
- Reduce or eliminate ground floor setback
- Incorporate unobtrusive or transparent weather protection (awning or canopy)

### Site 5

- Redevelopment has provided a continuous block face
- Generally good transparency at grade
- Canopy is too opaque and low to the

ground and overwhelming resulting in a darkened pedestrian environment

- Prominent corner entrance occupies the prime retail location
- Building massing and dark glass tint overwhelm the public realm environment

### Recommendations

- As it is recently redeveloped, no immediate action is recommended. Any future change will conform with the urban design guidelines

### Site 6

- Set of low scale buildings are in poor condition and contribute to a feeling of blight on the street
- Night club use results in a blank wall condition and unattractive appearance
- Balance of the block face excluding Audrey's Books should be comprehensively redeveloped

### Recommendations - Redevelopment

- Redevelop the site in conformity with urban design guidelines
- Ensure design that is responsive to the scale and massing of Audrey's Books

### Site 7

- Strong heritage character contributes well to the public realm
- Good transparency and visual rhythm at grade
- Active use

### Recommendations - Preservation

- Update weather protection with new awning or canopy

## North Side: Blocks 107-105



### Site 8

- Boston Pizza building is not of an urban character of scale and contributes to a suburban feel to the street
- Should be redeveloped
- Block is primarily vacant and in poor condition which contributes to a masse and dirty street environment

#### Recommendations - Redevelopment

- Block should be comprehensively redeveloped in accordance with the urban design guidelines

### Site 9

- Low ground floor height limits potential for high value retail
- Lack of active uses at grade
- Gold-tint facade is dated and unattractive

#### Recommendations - Major Retrofit

- Re-skin entire facade with translucent, non-tinted glazing
- Encourage two storey ground floor retail height to accommodate higher value retail
- Reduce or eliminate setback at ground level

### Site 10

- The retail units above and below grade result in poor quality street interaction and lack of accessibility
- Its low height reduces the quality of the street wall

#### Recommendations - Redevelopment

- Site should be redeveloped in accordance with urban design guidelines

### Site 11

- Generally an attractive building with positive street interaction

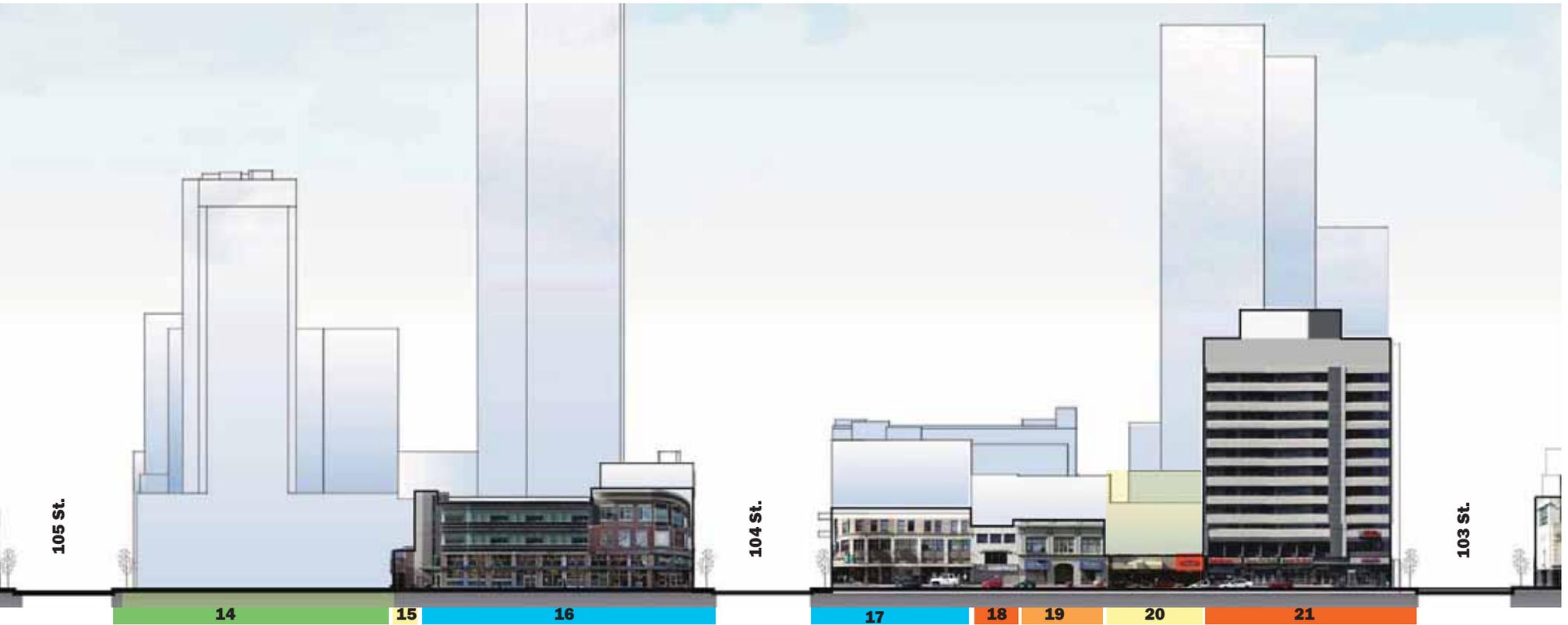
### Site 12

- Strong heritage character contributes well to the public realm
- Good transparency and visual rhythm at grade

### Site 13

- Tinted glass at grade reduces interaction between the inside retail use and the public realm
- Facade is generally dated looking
- Lack of definition between retail uses on ground floor and floors above
- Active retail use at grade

# North Side: Blocks 105-103



## Recommendations - Minor Retrofit

- Replace tinted glass at ground level with transparent, non-tinted glazing
- Update facade material to create more differentiation between upper and lower floors
- Incorporate unobtrusive or transparent weather protection (awning or canopy)
- Reduce or eliminate at-grade setback
- Raise ground floor height

## Site 14

- Not appropriately designed as an urban park

## Recommendations - Redesign

- Should be reimagined and redesigned to be a centrepiece to Jasper Avenue's public realm

## Site 15 (Lawyer Office)

- Provides an inactive edge to Beaver Hills House Park

## Recommendations - Redevelopment

- Site should be redeveloped to provide an active edge to the park per the urban design guidelines

## Site 16

- High quality new development
- Responds to adjacent heritage building with appropriate scale and materiality
- Provides a well articulated facade with good visual rhythm
- The grocery store provides an active and beneficial commercial use at grade
- Good use of the side street for patio space
- Good transparency at grade
- Well designed weather protection

## Site 17

- Good quality heritage building
- Canopy is too opaque, which produces a darkened pedestrian environment beneath

## Recommendations - Minor Update

- Replace opaque facade material at retail bays with transparent material
- Incorporate unobtrusive or transparent weather protection (awning or canopy)

## Site 18

- Building is in poor condition

## Recommendations - Major Retrofit

- Replace facade in compliance with urban design guidelines

## North Side: Blocks 103-101



### Site 19

- Facade is deteriorated and in need of repair
- Lack of active uses at grade
- Should be retrofitted

#### Recommendations - Minor Retrofit

- Renew facade material
- Replace canopy with unobtrusive or transparent weather protection (awning or canopy)
- Encourage active/retail uses at grade

### Site 20

- Building's low height reduces the quality of the street wall

- Good transparency at grade

- Active uses at grade

#### Recommendations - Redevelopment

- Redevelop in accordance with urban design guidelines

### Site 21

- Ground floor facade appears dated and unattractive
- Good transparency at grade
- Active uses at grade contribute positively to the public realm
- Lack of definition between the ground floor retail and office uses above

#### Recommendations - Major Retrofit

- Re-skin entire facade with greater proportion of translucent, non-tinted glazing material
- Encourage two storey ground floor retail height to accommodate higher value retail
- Create greater definition between upper and lower floors
- Incorporate unobtrusive or transparent weather protection (awnings or canopies)

### Site 22

- Positive adaptive reuse with good quality materials

- Block long building face with limited street entrances and inactive uses has a deadening effect on the street

#### Recommendations - Minor Retrofit

- Display windows in the short term should provide as much visual interest as possible
- Retrofit to introduce more active ground floor uses - create more retail bays and access points at grade

### Site 23

- Prominent building entrance provides a sense of arrival and a strong corporate address appropriate to the location

## North Side: Blocks 101-100



- Good transparency at grade
- Retail podium is well scaled and provides active uses

### Site 24

- Attractive heritage building contributes to an attractive street wall and pedestrian environment

### Site 25

- Facade is tired and dated
- Canopy is too opaque, low to the ground and overwhelming feeling to the pedestrian
- Good transparency at grade

- Active uses at grade contribute to street vitality

### Recommendations - Minor Retrofit

- Re-skin facade with a greater proportion of transparent material
- Replace canopy with unobtrusive or transparent weather protection (awnings or canopies)

### Site 26

- Podium configuration breaks down the linear quality of the street wall and creates awkward angles
- Lack of active ground floor uses
- Dated appearance

27

28

29

30

### Recommendations - Major Retrofit

- Structural retrofit would improve the podium's function and potential for high value retail
- Rebuild facade with a high level of transparency and high quality retail bays
- Reduce or eliminate podium setback at street level

### Site 27

- Facade is unattractive
- Lack of definition between retail uses at grade and office use above
- Active use at grade

### Recommendations - Minor Retrofit

- Update facade material conforming with urban design guidelines
- Utilize facade improvement to delineate upper floors from ground level
- Incorporate unobtrusive or transparent weather protection

### Site 28

- Good transparency at grade
- Generally dated and tired facade
- Good potential for active uses at grade
- Awkward transition between the first/second and upper floors

## North Side: Blocks 100-99



### Recommendations - Major Retrofit

- Structural retrofit to create stronger transition between second and upper floors
- Replace dark windows with transparent, non-tinted glazing material
- Incorporate unobtrusive or transparent weather protection (awnings or canopies)

### Site 29

- Ground floor conducive to high value retail and adaptability to new retail uses over time
- Good transparency at grade

- Well articulated facade
- Good weather protection canopy

### Site 30

- Primarily blank facade contributes poorly to the quality of the pedestrian environment
- Lack of active uses at grade
- Lack of definition between the upper and lower floors

### Recommendations - Redevelopment

- Redevelop site in accordance with urban design guidelines

### Site 31

- Attractive heritage property
- Only minor improvements needed

### Recommendations - Minor Retrofit

- Incorporate unobtrusive or transparent weather protection (awnings or canopies)

### Site 32

- Substantial vacant lot providing significant redevelopment potential

### Recommendations - Redevelopment

- Redevelop site according to urban design guidelines

### Site 33

- Ground floor height conducive to high value retail
- Good transparency at grade
- Cosmetic improvements would enhance retail function

### Recommendations - Minor Retrofit

- Retrofit ground floor to accommodate retail bays flanking bank lobby
- Update facade materials

### Site 34

- Is a relatively well-functioning and attractive private open space

## North Side: Blocks 97



### Site 35

- Currently being utilized as a private open space, but could potentially see redevelopment including the reconstruction of the historic Alberta Hotel

#### Recommendations - Redevelopment

- Redevelop site according to urban design guidelines

### Site 36

- Colonnade (arcade) creates a darkened and less active facade at grade, which detracts from the pedestrian environment

- Lack of active uses at grade
- Sunken plaza reduces sight lines and opportunities for natural surveillance

#### Recommendations - Major Retrofit

- Structural retrofit to improve relationship to public realm
- Replace colonnade with flush facade and unobtrusive weather protection
- Incorporate retail bays at grade
- Replace pink tinted glass with translucent, non-tinted glazing
- Redesign plaza to be flush with grade and pedestrian realm

## South Side: Blocks 97-99



### Site 1

- Primarily blank wall of Hall 'D' contributes to a lack of a pedestrian friendly environment on Jasper Avenue
- Public art components moderately enhance the public realm

### Recommendations - Minor Retrofit

- Cosmetic retrofit
- Replace opaque facade material with a greater proportion of transparent material OR material with greater visual interest
- Update public landscape, particularly the plaza west of entrance pavilion
- If possible, incorporate retail bays at grade
- Incorporate unobtrusive or transparent weather protection (awnings or canopies)

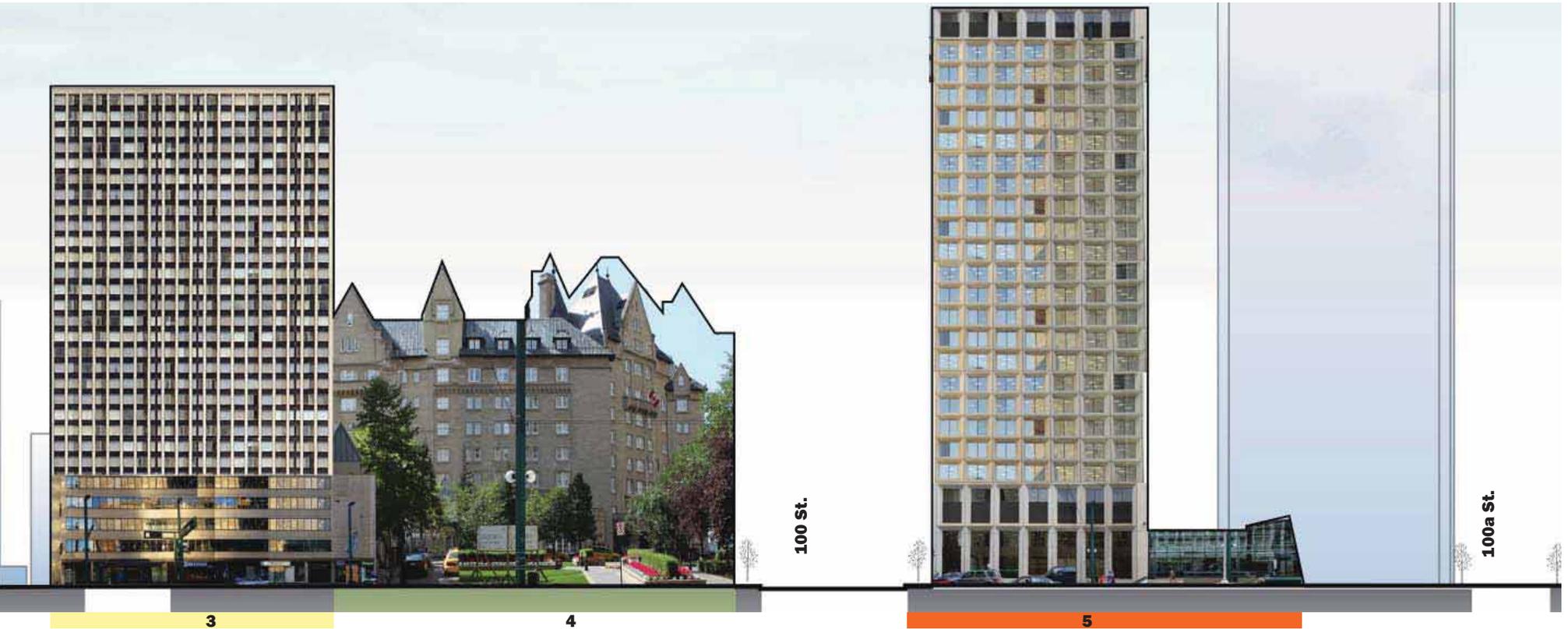
### Site 2

- Vacant site provides opportunity for redevelopment

### Recommendations - Redevelopment

- Complete redevelopment in accordance with urban design guidelines
- Site should be considered for redevelopment as a civic open space in conjunction with adjacent parcel

## South Side: Blocks 100-100A



### Site 3

- Unattractive and dated building does not contribute to a positive pedestrian experience
- Site should be considered for redevelopment as a civic open space in conjunction with adjacent parcel

#### Recommendations - Redevelopment

- Acquire and redevelop site potentially with a public open space in accordance with urban design guidelines

### Site 4

- Privately held open space that should be preserved permanently as open space and redeveloped in conjunction with the adjacent parcel
- Open space functions as a formal garden forecourt to the Fairmont Hotel Macdonald

#### Recommendations

- Acquire site

### Site 5 (Telus Tower)

- Street interface is hostile to the pedestrian realm
- Ground floor is an office use which fails to animate the pedestrian realm
- The tower meets the sidewalk in a blunt manner
- Private plaza provides some green amenity to the commercial core

#### Recommendations - Major Retrofit

- Ground floor should undergo a structural retrofit to incorporate street-fronting commercial retail units
- Incorporate more entry points at grade
- Ensure facade improvement provides transparency and weather protection measures in accordance with urban design guidelines

## South Side: Blocks 100a-102



### Site 6

- Heritage building positively contributes Jasper Avenue's public realm

### Site 7

- Building provides active uses at grade
- Good transparency at grade
- Good definition between the lower and upper floors of the building
- Cosmetic retrofit appears dated and quality of materials should be upgraded

#### Recommendations - Minor Retrofit

- Upgrade ground level facade with lighter coloured, higher quality materials
- Incorporate unobtrusive or transparent

weather protection (awnings or canopies)

### Site 8

- Primarily blank facade contributes poorly to the quality of the pedestrian environment
- Lack of active uses at grade
- Lack of definition between the upper and lower floors

#### Recommendations - Major Retrofit

- Structural retrofit to accommodate street-related retail uses
- Update facade with higher quality material in accordance with urban design guidelines

- Incorporate unobtrusive or transparent weather protection (awnings or canopies)

### Site 9

- Only a small portion of the podium accommodates active uses
- Poor transparency due to window blinds at grade throughout much of the facade
- Podium and set back tower provides a human scale built form

#### Recommendations - Major Retrofit

- Structural retrofit to accommodate street-related retail uses surrounding bank lobby

### Site 10

- Privately held open space that may be redeveloped
- Existing open space provides some green amenity to the commercial core

#### Recommendations - Redevelopment

- Redevelop site per urban design guidelines

### Site 11

- Primarily blank facade contributes poorly to the quality of the pedestrian environment
- Lack of active uses at grade
- Lack of definition between the upper and lower floors

## South Side: Blocks 102-104



### Recommendations - Major Retrofit

- Retrofit to create retail bays for active uses at street level
- Update facade to include more differentiation between upper and lower floors
- Incorporate unobtrusive or transparent weather protection (awnings or canopies)

### Site 12

- Site to be redeveloped
- Paramount theatre facade exhibits an interesting example of mid-century modern architecture

### Recommendations - Redevelopment

- Ensure proposed development scheme complies with urban design guidelines

### Site 13

- Corner entrance breaks down the continuous street wall condition and does not provide active uses
- Western portion of the podium provides active uses that contribute positively to the pedestrian realm and are transparent

### Recommendations - Major and Minor Retrofit

- Structural retrofit of the eastern portion to remove corner entrance and

### accommodate active uses

- Update western facade, including unobtrusive or transparent weather protection

### Site 14

- Building's low height reduces the quality of the street wall
- Good transparency at grade
- Active uses at grade

### Recommendations - Redevelopment

- Site should be considered for redevelopment, potentially with adjacent parcel to the west

### Site 15

- Building appears dated
- Lack of definition between the upper and lower floors

### Recommendations - Major Retrofit

- Structural retrofit to improve the capacity for high value retail at grade
- Encourage two storey ground floor retail height to accommodate higher value retail
- Update facade material, create greater differentiation between upper and lower floors
- Include unobtrusive or transparent weather protection (canopy or awning)

## South Side: Blocks 104-106



### Site 16

- Podium and tower form generally contributes to a human scale pedestrian experience
- Good definition between the lower and upper floors of the building
- Good transparency at grade
- A high proportion of the ground floor is occupied by inactive uses, which do not contribute to the vitality of the street
- Few direct entrances into ground floor from the street
- Recessing of the ground floor facades contributes to a dark and uninviting pedestrian realm and from the

pedestrian perspective creates voids in the street wall

#### Recommendations - Minor Retrofit

- Should undergo a retrofit of podium to accommodate high value retail uses
- Reduce or eliminate recess at ground floor
- Include more street-front entrances
- Upgrade the building's appearance through new facade material and light or transparent weather protection

### Site 17

- Primarily blank facade contributes poorly to the quality of the pedestrian environment

- Lack of active uses at grade

#### Recommendations - Redevelopment

- Should be redeveloped, potentially with adjacent parcel to the west

### Site 18

- Building's low height reduces the quality of the street wall
- Good transparency at grade
- Active uses at grade
- Building appears in poor condition

#### Recommendations - Redevelopment

- Site should be considered for redevelopment, potentially with adjacent parcel to the east

### Site 19

- Ground floor uses do not meet the grade of the sidewalk, which creates a blank wall and contributes to a hostile pedestrian environment

- Grade change reduces the accessibility of ground floor uses

- Poor transparency

#### Recommendations - Major Retrofit

- Building should undergo a structural retrofit to bring ground floor uses to grade
- Update facade with a greater proportion transparent material
- Encourage two storey ground floor retail

# South Side: Blocks 106-108



- height to accommodate higher value retail
- Include unobtrusive or transparent weather protection (canopy or awning)

**Site 20**

- Vacant parcel presents redevelopment opportunity

**Site 21**

- Ground floor retail is sunken below grade and is inaccessible from directly from the street, which contributes to a hostile pedestrian environment
- Poor transparency

- Corner plaza breaks down streetwall condition
- Recommendations - Major Retrofit
- Building should undergo a structural retrofit to bring the ground floor to grade
  - Upgrade facade to include a greater proportion of transparent material
  - Western plaza could be redeveloped to incorporate LRT entrance into the building

**Site 22**

- Generally good transparency at grade
- Structure of the building is conducive to active ground floor uses, however the bank at the corner location is not an

- active use
  - Lack of definition between lower and upper floors of the building
- Recommendations - Minor Retrofit
- Building should undergo cosmetic upgrade to improve the appearance of the retail units including new glazing, signage and potentially facade materials to better accommodate high value retail uses
  - Relocate bank lobby to mid-block to accommodate high value retail at corner

**Site 23**

- Vacant site to be redeveloped

- Recommendations - Redevelopment
- Ensure development plans conform with urban design guidelines
  - Ensure scale and form of new development complement existing building to the west

**Site 24**

- Generally an attractive building
- Night club use is inactive throughout many parts of the day

Recommendations - Redevelopment

- Ensure redevelopment plans conform with urban design guidelines

## South Side: Blocks 108-110



### Site 25

- Site to be comprehensively redeveloped
- Recommendations - Redevelopment
- Ensure development plans conform with urban design guidelines

### Site 26

- Ground floor uses do not meet the grade of the sidewalk, which creates a blank wall and contributes to a hostile pedestrian environment
- Grade change reduces the accessibility of ground floor uses
- Poor transparency
- Building appears dated

### Recommendations - Major Retrofit

- Building should undergo a structural retrofit
- Encourage two storey ground floor retail height to accommodate higher value retail
- Update facade with transparent, non-mirrored glazing and light materials
- Incorporate light or transparent weather protection (canopies or awnings)

### Site 27

- Vacant site to be redeveloped
  - Challenging grade issues
- Recommendations - Redevelopment
- Ensure development plans conform with urban design guidelines

## South Side: Blocks 110-111



### Site 28

- Ground floor uses do not meet the grade of the sidewalk, which creates a blank wall and contributes to a hostile pedestrian environment
- Grade change reduces the accessibility of ground floor uses
- Poor transparency
- Building appears dated

#### Recommendations - Major Retrofit

- Building should undergo a structural retrofit
- Encourage two storey ground floor retail height to accommodate higher value

### retail

- Update facade with transparent, non-mirrored glazing and light materials
- Incorporate unobtrusive or transparent weather protection (canopies or awnings)

### Site 29

- Ground floor uses do not meet the grade of the sidewalk, which creates a blank wall and contributes to a hostile pedestrian environment
- Grade change reduces the accessibility of ground floor uses
- Poor transparency

- Building appears dated

#### Recommendations - Major Retrofit

- Building should undergo a structural retrofit
- Encourage two storey ground floor retail height to accommodate higher value retail
- Update facade with transparent, non-mirrored glazing and light materials
- Incorporate unobtrusive or transparent weather protection (canopies or awnings)



## **Appendix B**

Jasper Avenue Block by Block Streetscape  
Concept Plan



# Block by Block Jasper Avenue Streetscape Concept Plan

## 111th to 110th



Figure 33. Concept Plan 111th to 110th

# 110th to 109th



Figure 34. Concept Plan 110th to 109th

## 109th to 108th



Figure 35. Concept Plan 109th to 108th

## 108th to 107th



Figure 36. Concept Plan 108th 107th

## 107th to 106th



Figure 37. Concept Plan 107th to 106th

## 106th to 105th



Figure 38. Concept Plan 106th to 105th

## 105th to 104th



Figure 39. Concept Plan 105th to 104th

## 104th to 103rd



Figure 40. Concept Plan 104th to 103rd

## 103rd to 102nd



Figure 41. Concept Plan 103rd to 102nd

# 102nd to 101st



Figure 42. Concept Plan 102nd to 101st

## 101st to 100A



Figure 43. Concept Plan 101st to 100A

## 100A to 100th



Figure 44. Concept Plan 100A to 100th

## 100th to 99th



Figure 45. Concept Plan 100th to 99th

# 99th East



Figure 46. Concept Plan 99th East

# 97th West

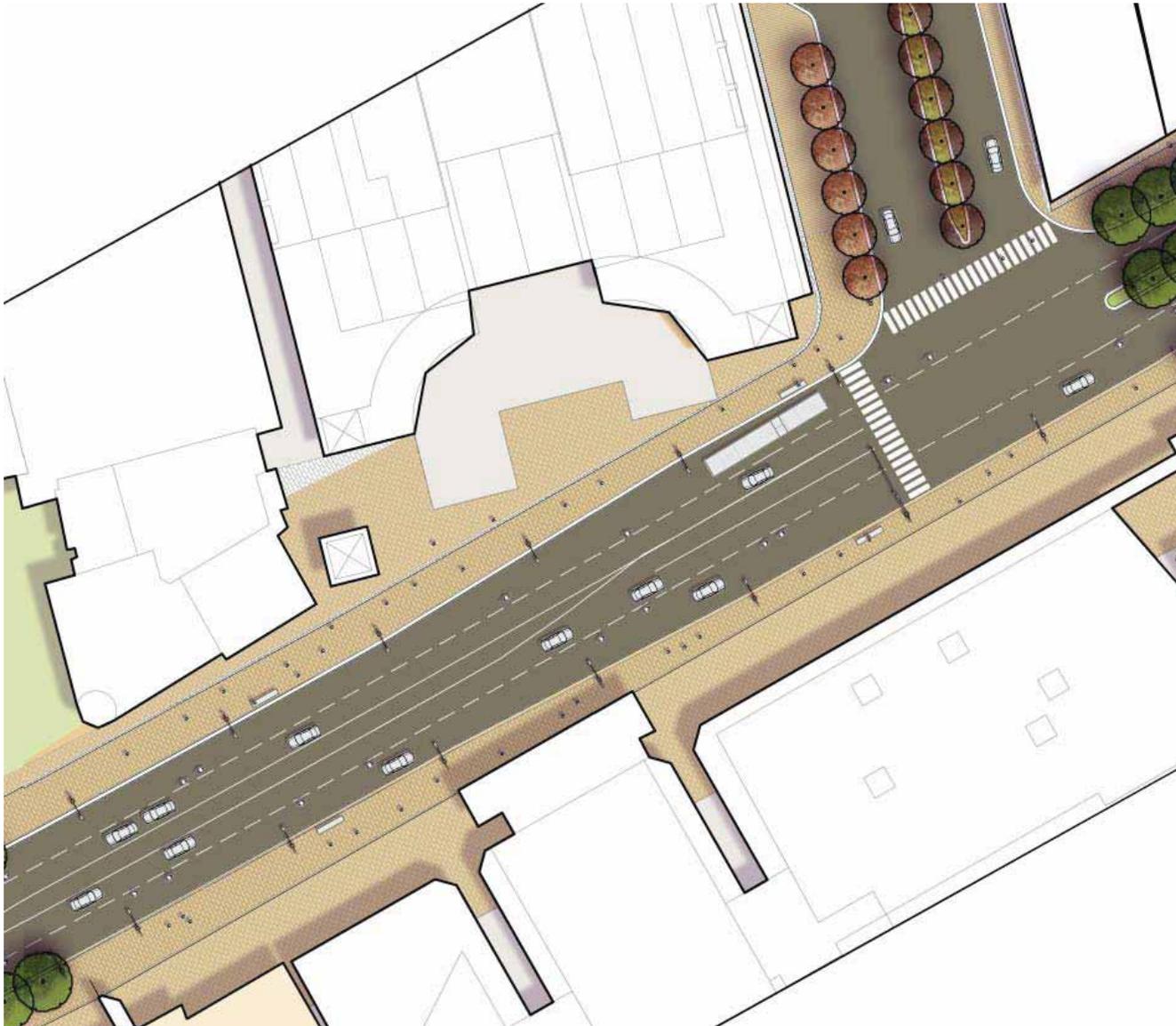


Figure 47. Concept Plan 97th West



## **Appendix C**

Jasper Avenue Demonstration Elevation - North Side



Demonstration Jasper Avenue  
Streetscape North-Side Elevations

**111th to 110th**



Figure 48. Demonstration North-Side Elevation 111th to 110th

## 110th to 109th



Figure 49. Demonstration North-Side Elevation 110th to 109th

## 109th to 108th



Figure 50. Demonstration North-Side Elevation 109th to 108th

## 108th to 107th



Figure 51. Demonstration North-Side Elevation 108th to 107th

## 107th to 106th



Figure 52. Demonstration North-Side Elevation 107th to 106th

## 106th to 105th



Figure 53. Demonstration North-Side Elevation 106th to 105th

## 105th to 104th

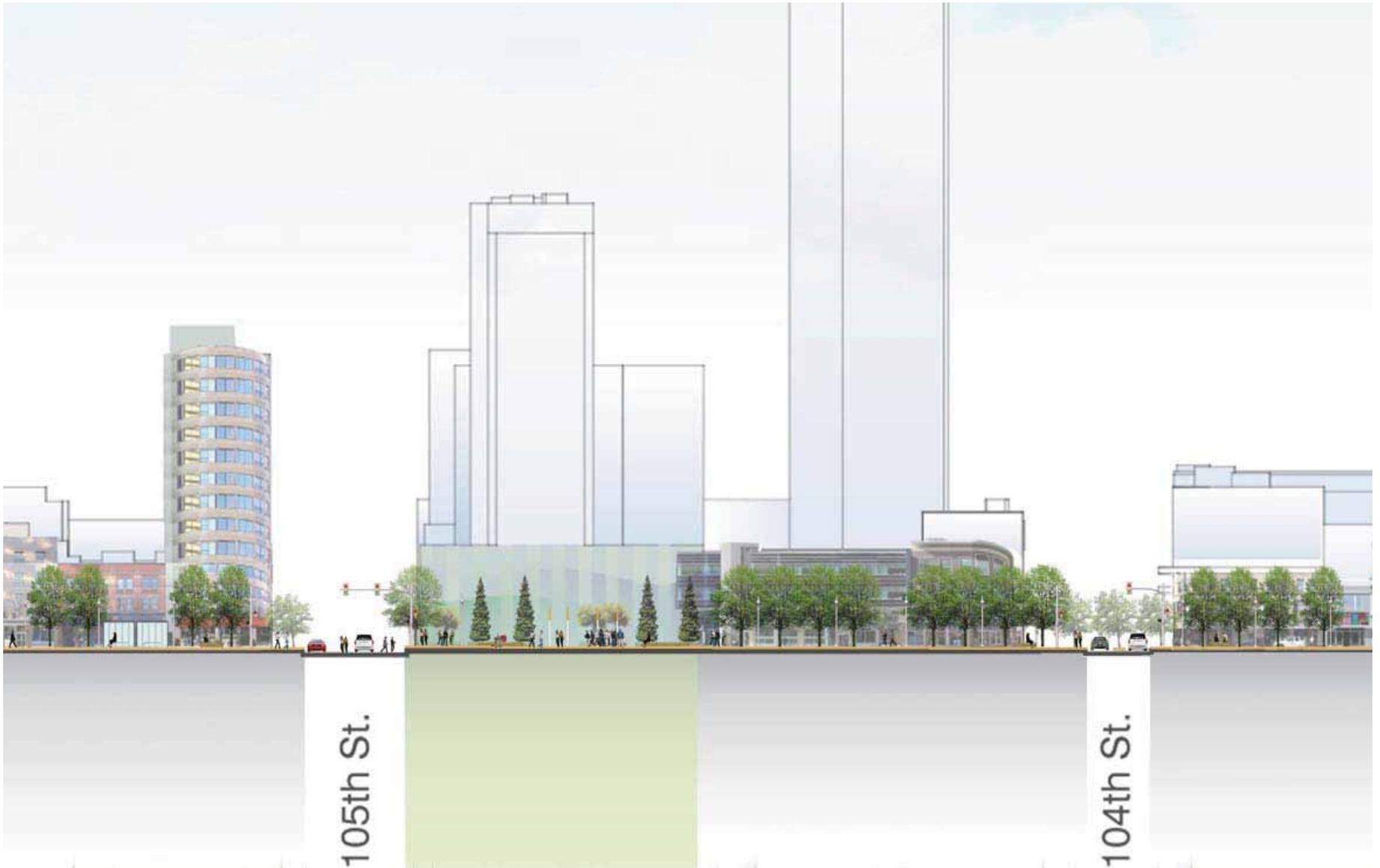


Figure 54. Demonstration North-Side Elevation 105th to 104th

# 104th to 103rd

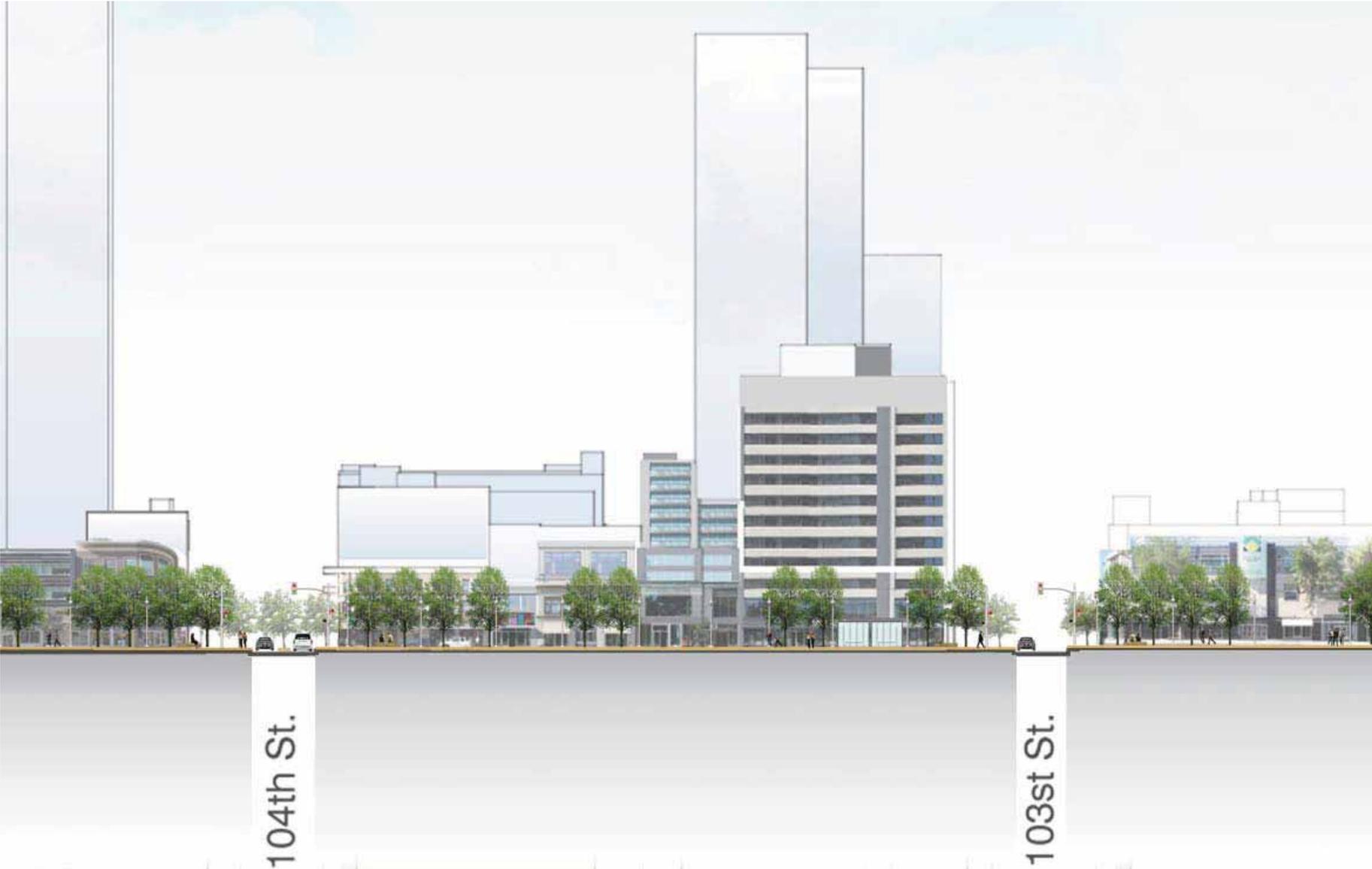


Figure 55. Demonstration North-Sdie Elevation 104th to 103rd

# 102nd

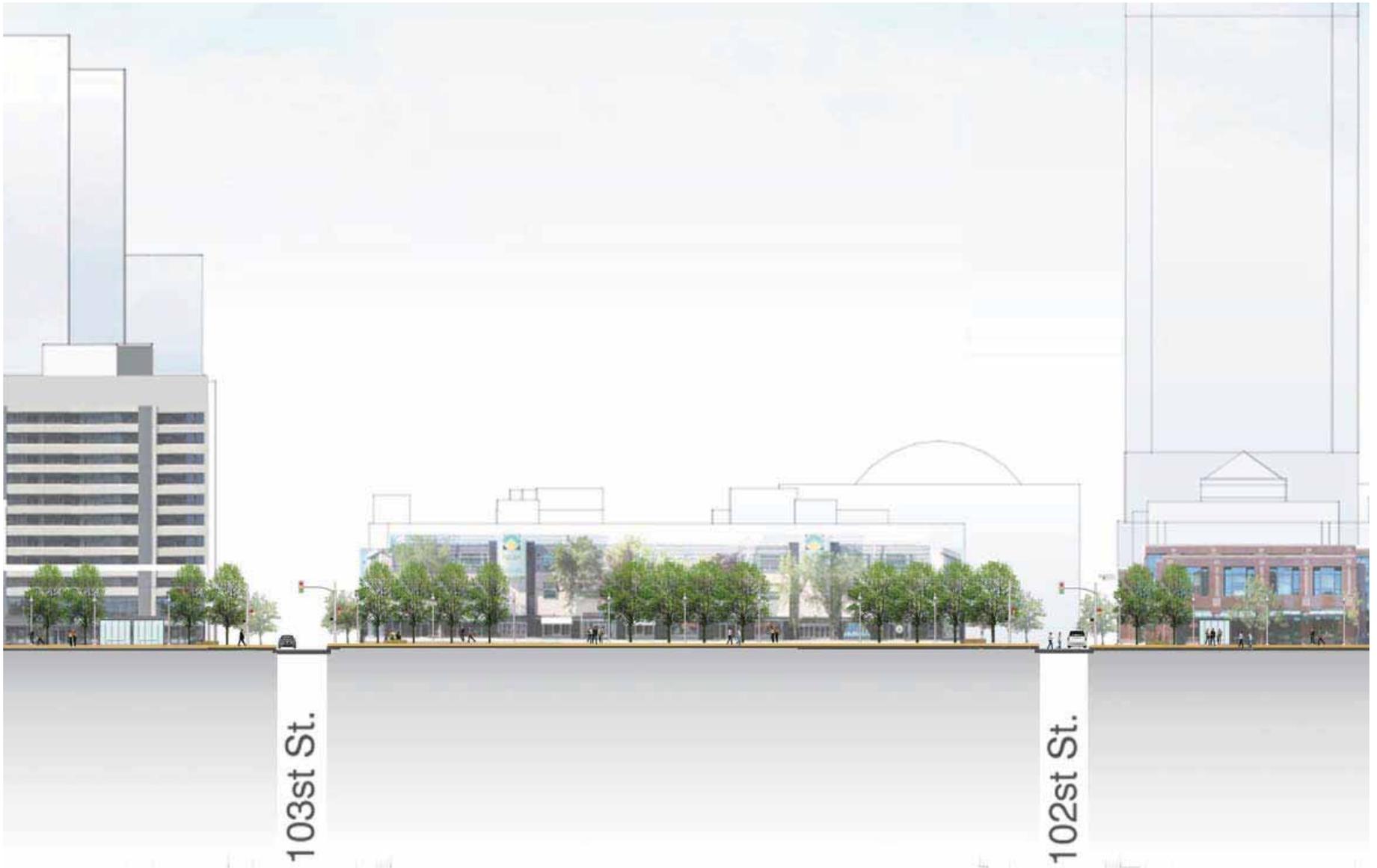


Figure 56. Demonstration North-Side Elevation 102nd



## **Appendix D**

Additional Information



## Great Streets

### **In Toronto, Bloor Street, between Church Street and Avenue Road, is undergoing a \$30 million dollar Streetscape Improvement to reposition the street as an international shopping destination.**

This initiative has been lead by the Yorkville Business Improvement Association (BIA), with financial contributions by the City, Province and members of the BIA to transform 920 meters of Bloor Street in the vicinity of the Yonge Street Intersection. This part of Bloor street has seen extensive mixed use high density redevelopment over the last 15 years which has changed the nature of the retail merchandizing mix toward a mid to high end flagship retail offering now including stores such as Cartier, Tiffany's, Louis Vuitton, Prada and Coach. The streetscape initiative is expected to transform the physical setting offering a higher quality pedestrian experience to complement the emerging retail offering. Large scale mixed use development is also accelerating along the Bloor Street corridor, including a proposed 65 storey condominium tower at the intersection of Bloor and Yonge.

The streetscape initiative will be complete in 2011 and involves:

- Reconfiguration of the Right-of-Way to narrow the roadway to four narrower lanes (2 lanes in each direction/1 lane in each direction with off peak parking);
- Widening the pedestrian walking area by a minimum 2.5 metres on either side of the street resulting in a 6 to 10 m sidewalk area;
- Use of large size Granite pavers, Granite curbs and extensive landscaping including large Calliper street trees; and
- Mixed traffic bicycle movement (no dedicated on-street bicycle lanes).

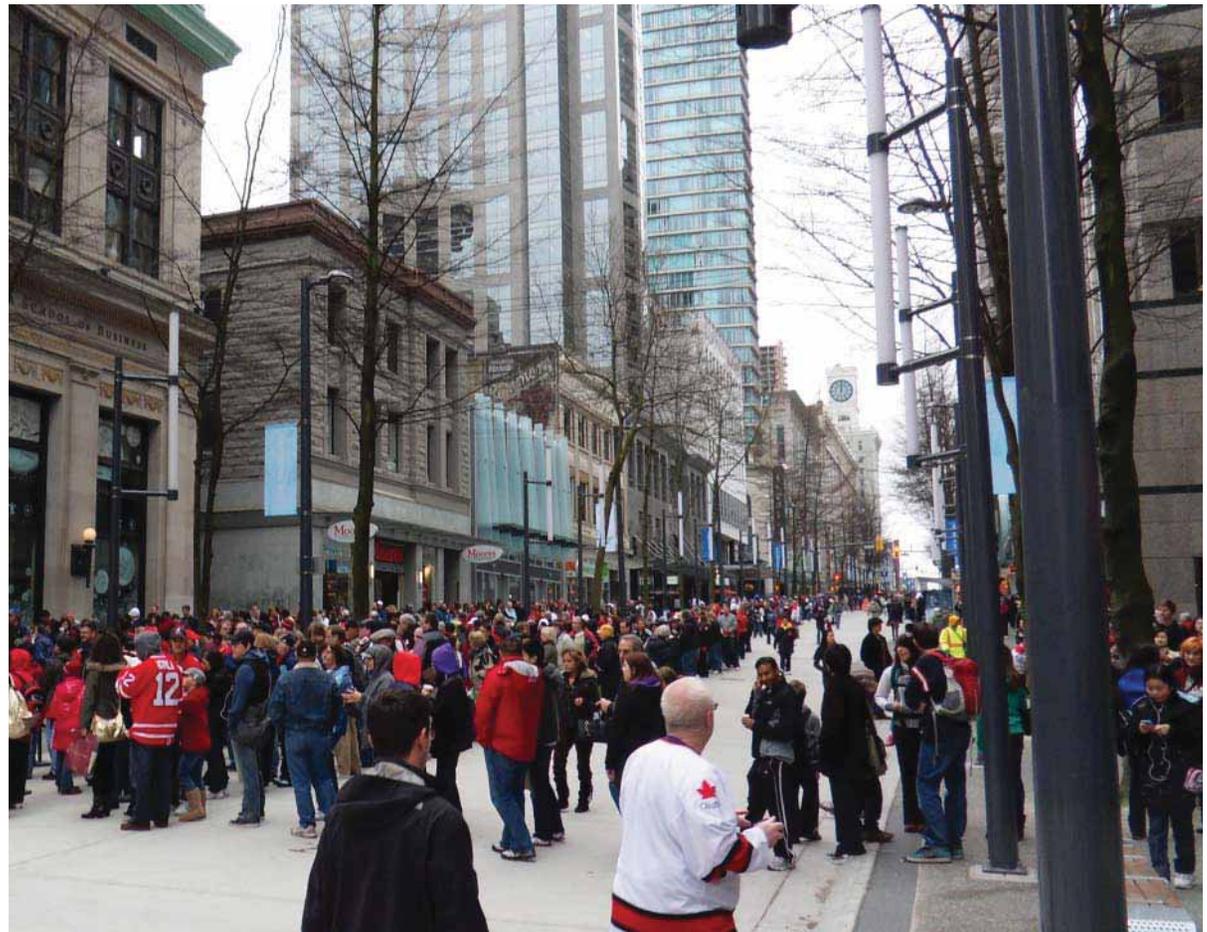
### **In Vancouver, Granville Street, between Drake and Cordova Streets, is undergoing a \$21 million steetscape Improvement.**



Granville is one of Vancouver's signature streets and includes some of the city's most well known performance venues including the Orpheum Theatre as well as a number of shopping, restaurant and night-life establishments. It is one of Vancouver's original main streets. The construction of the 1.5 km long streetscape revitalization project coincided with the development of the Canada Line, a new rapid transit line connecting downtown Vancouver to International Airport and the City of Richmond.

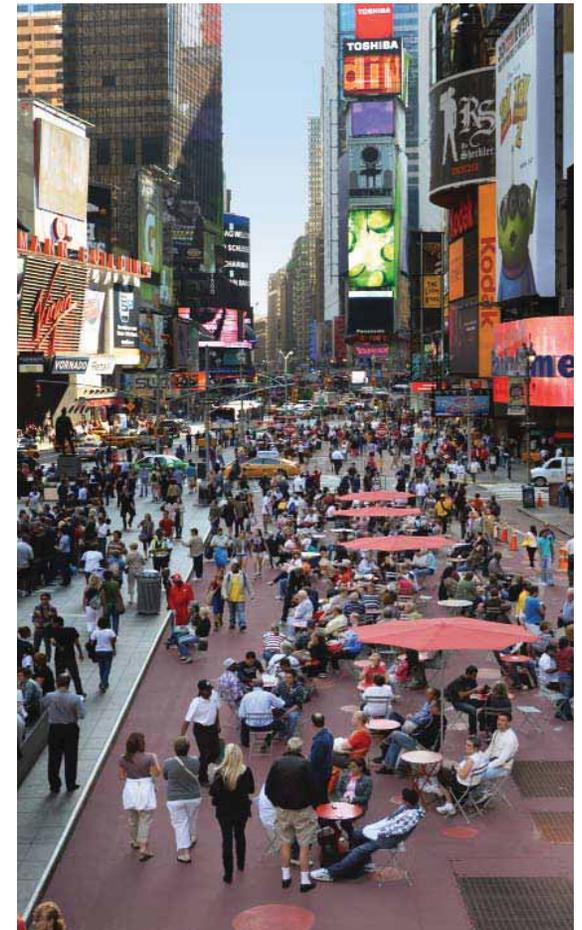
The Streetscape project was completed in early 2008 and involves:

- Reconfiguration of the Right-of-Way to narrow roadway and widen pedestrian realm;
- New paving, street furniture, lighting and tree planting;
- The ability to be closed to automobile traffic during events. Granville became a pedestrian-only street during the 2010 Olympic Games and is planned to close to traffic for special events.



**In New York, Broadway is proposed to be permanently closed through the Times Square Area.**

The New York City Department of Transportation, as part of an aggressive strategy to improve the pedestrian and cycling environment and to improve an array of important Manhattan public spaces, proposed to close Broadway Avenue between 42nd and 47th Streets, creating new public plaza spaces where the roadway currently exists. Following a temporary trail, the concept proved so successful that the City is in the process of closing Broadway and converting the space into a permanent plaza.





## **Appendix E**

Historic Overview of Jasper  
Avenue



## Historic overview of Jasper Avenue

### Pre-WWII

Jasper Avenue has a unique and interesting history which is tied to the evolution and growth of western Canada. Prior to 1870, Fort Edmonton was the primary settlement in the area and functioned as an important trading, distribution and supply centre for the northwest on the Hudson Bay Company's trans-Canada route.

After 1870, when the region became part of the new Dominion of Canada, legislation made it possible for private individuals to own land that had formerly belonged exclusively to the Hudson Bay Company and, on both sides of the North Saskatchewan River, settlements soon emerged as a separate entities outside the secure boundaries of the Fort.

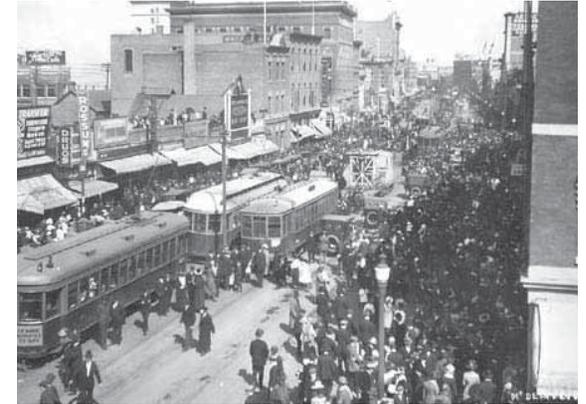
1900s



South Edmonton (Old Strathcona) emerged to the south, with Whyte Avenue and 103rd Street as its commercial hub, and Edmonton evolved on the north when the Hudson Bay Company began selling portions of its land grant.

Jasper Avenue evolved as a new commercial main street between 96th and 99th Streets, a place for traders dealing in dry goods, furs and also as a service centre for surrounding agricultural lands. By 1881, additional land subdivision expanded the Avenue and within 10 years, brick, concrete, and steel had become the standard building materials. In a very short time period, Edmonton took on the appearance of a mature city and Jasper Avenue was a prosperous and active main street, with prestigious brick and stone buildings on multiple floors. Dirt streets and wood plank

1920s



boardwalks attest to the rapid pace of growth during this period.

Meanwhile, to the south of the river, South Edmonton grew with the completion of the Canadian Pacific Railway line linking Calgary to the south banks of the river in 1891. South Edmonton, centered on what is now 103rd Street and Whyte Avenue, would soon overtake Edmonton.

By the turn of the 20th Century, when the Canadian Northern and the Grand Trunk Pacific Railways steamed onto the Edmonton's north side of the river, the Canadian Pacific Railway quickly built the High Level Bridge to bring their own railway to Edmonton. Development and growth then shifted to the north side of the river.

1930s



1940s



In 1904 Edmonton was incorporated as a city and became the provisional capital of the new province of Alberta in 1905, and official capital in 1906. Strathcona was incorporated as a city in 1907 and became the site of the University of Alberta the same year. The rivalry between the cities ended in 1912 when continuing pressure for infrastructure and services compelled citizens of Strathcona to vote for amalgamation with Edmonton. By 1914, the city of Edmonton had grown to 70,000, a dramatic change from the near 3000 at the turn of the century.

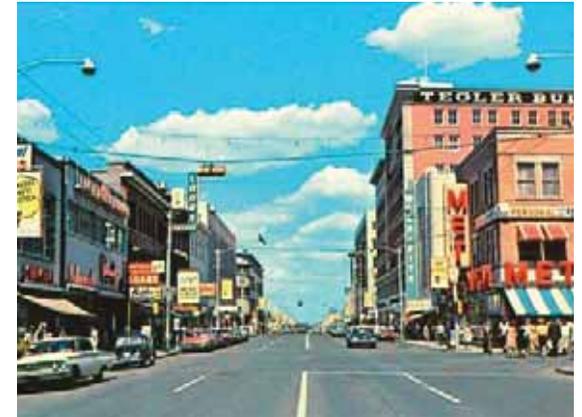
By the 1920's, Jasper Avenue had become the bustling heart of the city. Despite the economic woes of the depression, Jasper Avenue continued to evolve as the centre of commerce and business in the city and

streetcars and motorized vehicles replaced the earlier horse and cart. People and activity flowed into the corridor creating a vibrant commercial and mixed use core for the city. By the mid 1930s, new stone and brick buildings had replaced the last of the one and two story wood and clapboard structures elevating Jasper Avenue to Edmonton's most prestigious business address.

1950s



1960s



## Post-WWII

The prosperity and growth of the post-war era marked a turning point for Jasper Avenue. With the discovery of oil in Leduc in 1948, and Edmonton positioned as an operations and staging base for large construction projects including the construction of the Alaska Highway and the Canol pipeline, the city entered a new post-war cycle of economic and physical growth. The city boundaries were expanded, as Beverly (1961) and Jasper Place (1964) were annexed. Suburban growth flourished and new construction began in the downtown core.

The City of Edmonton prospered through the 1960s and '70s as the oil boom brought thousands of jobs and newcomers. Downtown was transformed through 'urban renewal projects' and land speculation which removed many historic buildings replacing them with modern structures, corporate offices uses and of course, parking lots.

While Jasper Avenue remained the primary shopping focus for the city up to the early 1970s, including large department stores such as The Bay and Woodwards, decentralized development patterns, rapid suburban expansion and the emergence of new 'shopping centres' such as Edmonton Centre, Kingsway, Southgate, Westmount and others would gradually pull people and activity away from Jasper Avenue. In addition, there was widespread demolition of main street fabric for the creation of new parking lots developed as placeholders for future development when economic conditions were 'right'. This compromised the character and sense of place of the Avenue, lessening its ability to effectively compete with new indoor shopping environments. Throughout the late 70s and early 80s corporate office towers such as Manulife Place, Telus Plaza, TD Tower and Scotia place, changed the complexion of downtown, and Jasper Avenue, shifting in emphasis from street related retail to corporate office and indoor retail, most notably east of 102nd Avenue.

Perhaps the most devastating blow to the Avenue occurred in the early eighties, with the opening of the West Edmonton Mall, the largest shopping mall in the world at the time. Shortly after this, it became apparent that the commercial and retail draw of Jasper Avenue was in significant decline. Retail business on the street was poor and several merchants moved to more profitable locations. Retail rents began to drop and bars and night clubs moved into the vicinity of 108th Street, bringing a myriad of policing and late night disturbance related incidents to the street.

In 1985, the City sought to curb further decline of the street through a streetscape revitalization project and the construction of the LRT below the street, both of which were positive moves that helped Jasper Avenue maintain a foothold as an important street within Edmonton. Throughout the 90s development was slow, however significant institutional projects including Edmonton's new City Hall (1992), Grant MacEwan University's downtown campus (1993), and the Winspear Centre(1997) built some momentum in

West Edmonton Mall



the downtown. Continued suburban expansion and the emergence of vast tracts of large format retail Power Centres at the edges of the city challenged Jasper Avenue's viability as a destination shopping street.

In the late 90s and 2000s, signs of revitalization have occurred in and around Jasper Avenue. Railtown, the Icon Towers and an array of residential projects increased the downtown population bolstering the number of people living on and supporting the Avenue. New commercial uses such as the Sobeys at 104th Street have demonstrated that there is a market and demand for more downtown shopping. The recently renovated AGA further enhances the cultural and destination draw of the downtown. However, the infrastructure and streetscape of the mid 80s, contemporary in its time, has reached the end of its lifecycle, appears dated and deteriorated. Portions of the street are well known as late night entertainment destinations and contribute to a perception that the area is unsafe at night. Commercial reinvestment and an improved commercial merchandising mix are needed

and a permanent residential population is missing which could champion positive change and make the area populated in both the day time and evening.

While Jasper Avenue changed significantly in the post World War Baby Boom, Whyte Avenue has thrived in recent years. While it also endured cycles of decline and stagnation, a lack of development pressure largely preserved its original urban character and heritage buildings giving Whyte Avenue a strong foundation for its current renewal and emergence as a hip and interesting leisure destination. As a result, Whyte Avenue has usurped Jasper Avenue as the destination of choice within Edmonton for civic celebration, as evidenced in the Oilers 2006 Stanley Cup Final run as well as important cultural events such as the Fringe Festival.

Today, Edmonton's population has topped 750,000 and the Edmonton region population exceeds 1 million making Edmonton Canada's sixth largest metropolitan area. The City has decided that creating a new urban design vision and streetscape improvement plan for Jasper Avenue is a priority.